

# Technical Appendix 14.1: Preliminary Access Management Plan (PAMP)

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## 1.0 Introduction

### 1.1 Background

This document presents a Preliminary Public Access Management Plan (PAMP) for the Oliver Forest Wind Farm (hereafter referred to as 'the Proposed Development') which outlines the principles and procedures which would be put in place to address any potential impacts to public access during the construction of the Proposed Development. It also outlines provisions for public access management during the operational life of the Proposed Development, as well as aspirations for future long-term access to the wider area.

This document should be read in conjunction with the following documents that form part of the Oliver Forest Wind Farm Environmental Impact Assessment (EIA) Report:

- Chapter 1: Introduction;
- Chapter 3: Description of the Development;
- Chapter 12: Site Access, Traffic and Transport;
- Chapter 14: Socio-economics, Recreation and Tourism;
- Technical Appendix 3.1: Outline Construction Environmental Management Plan (CEMP); and
- Technical Appendix 8.5: Outline Nature Enhancement Management Plan (NEMP).

This PAMP is not meant to be a formal Outdoor Access Plan (OAP) and, therefore, has not been drafted in line with NatureScot's OAP Guidance.

### 1.2 Purposes, Aims and Objectives

The purpose of this document is to outline how the Applicant would manage public access during the construction and operation phases of the Proposed Development. The PAMP accompanies the EIA Report for the Proposed Development and, should consent be granted, it is envisaged that this PAMP would be revised and updated to form an Access Management Plan (AMP) following discussion with Scottish Borders Council (SBC), NatureScot and the relevant landowners, to ensure that the aims, objectives and details meet the stakeholder's requirements.

It is envisaged that the finalised AMP for the Proposed Development would be secured and enforced via a planning condition.

The final AMP is intended to be deliverable, to safeguard access during construction, provide access enhancements during the operational phase of the Proposed Development and facilitate opportunities for improving access in the local area surrounding the Proposed Development. It would aim to address the basic infrastructure associated with improving access such as car parks, trailheads<sup>1</sup> and path improvement, inclusive of accessible path within the Proposed Development and would set up a framework for delivering improved access arrangements throughout the life of the Proposed Development.

This PAMP does not provide exact details of signage and interpretation to be implemented on-site, but instead it provides outline examples or suggestions for further discussion and agreement with consultees. If the Proposed Development is consented the AMP would be further developed to agree a number of specific details with the SBC Access Officer, such as path surfacing and specification.

This PAMP aims to cover and / or include outline specifications for the following:

- access management during construction, including the rights of access and where necessary, the provision of alternative routes:
  - these would be defined on the ground by maps and signage;
- longer term access proposals, including:
  - details of the arrangements to be made for enabling and improving access rights, e.g. paths and trails, and signs within the Proposed Development;
  - enhancement of existing routes within the Proposed Development and creation of a new recreational heritage trail;

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<sup>1</sup> The point at which a path begins and includes for signage / signposting.

- the specification for any new paths, some of which would be constructed to a multi-use standard;
- the specification of access controls (e.g. location of locked gates), which should also make provision for non-motorised users;
- the location, design, and layout of car parking facilities;
- the location, design and layout of access points and path networks for pedestrians and other users;
- the location and details of any interpretative / information facilities including mapping at appropriate road heads and any other visitor facilities to be provided (e.g. bins at car park);
- provisions for maintenance of any existing, upgraded, or new routes; and
- outline the opportunity for the path system developed as part of the Proposed Development to be linked with the wider path network, providing long distance walking and cycle routes, including:
  - details of the timescales for implementing all of these measures.

This last point would be included within the final AMP, if permission is granted and stakeholder discussions are advanced.

## 2.0 Site Location

The site is located in the Tweed Valley in the Southern Uplands of Scotland. The site is dissected by the A701 with the Proposed Development infrastructure located to the north of the A701; and the southern area proposed to be used for biodiversity enhancement measures. The site measures approximately 350 hectares (ha) and is comprised of commercial plantation forestry (managed by RTS Forestry) and open moorland, spread across the extent of three named hills, Upper Oliver Dod (490 m Above Ordnance Datum (AOD)); Weird Law (447 m AOD); and Glenmuck Height (472 m AOD). Elevations within the site decrease steeply from the north-east peak of Upper Oliver Dod to the River Tweed (south-east), at approximately 260 m AOD. The terrain is distinctly more elevated north-west of the site at Gathersnow Hill (688 m AOD) and Coombe Hill (640 m AOD).

The portion of the site located to the south-east of the A701 adjoins the River Tweed. A number of small tributary watercourses flow into the Tweed from the site including the Gala Burn, Rigs Burn, Bield Burn, Long Slack and Hallow Burn. Other tributaries, including the Menzion Burn and Glenrusco Burn, drain into the Tweed Valley from the adjacent slopes of Cockiland Hill, Muckle Knowe Hill and the Rig. The Fruid, Talla and Meggett Reservoirs lie at approximate distances of 2 km, 1.7 km and 8 km (respectively) to the south and east of the A701.

The immediate surrounding area of the site is rural in nature, and residential properties are restricted to the village of Tweedsmuir and the lower elevations of the Tweed Valley in proximity of the A701. There are several residential properties clustered just north of the four-way road junction at Tweedsmuir, east of the site boundary, and several others between Bield Burn and Tweedsmuir, north-east of the site boundary.

Outwith Tweedsmuir, the remote setting of the site means there are no other settlements within 10 km of the site. The village of Broughton is located approximately 12.5 km north of the site and provides services and facilities to the local area including a post office, village store and Broughton Primary School. The town of Moffat in Dumfries and Galloway (approximately 19 km south) according to the National Records of Scotland, has a population in the region of 2,460 and is separated by the expansive upland hills comprising the Southern Uplands. The transport network in the wider vicinity of the site and Southern Uplands are very much limited to the A74 motorway, A702, A701 and A708. These major roads all roughly follow a west south-west, east south-east alignment from Dumfries to Edinburgh and Selkirk, through the centre of the Southern Uplands range.

There are no statutory ecological designations within the site boundary. However, the River Tweed, which adjoins the site and into which tributaries of the site drain, is a Special Area of Conservation (SAC). Atlantic salmon, otter and aquatic vegetation are a primary reason and lamprey a qualifying reason for this designation status. The River Tweed is also designated as a Site of Special Scientific Interest (SSSI). Furthermore, salmon and lamprey are UK Biodiversity Action Plan species listed as priorities for conservation.

Other nearby natural heritage designations (within 10 km) include:

- Tweedsmuir Hills SSSI, approximately 2.2 km to the north-east;
- Craigdilly SSSI, approximately 8.8 km to the south-east; and
- Moffat Hills SAC, approximately 9.2 km to the south-east.

There are three Scheduled Monuments within the site boundary:

- Weird Law, Platform Settlement (SM3529).
- Menzion Farm, Enclosed Cremation Cemetery (SM2702).
- Menzion Farmhouse, Two Enclosed Cremation Cemeteries (SM2748).

A number of Scheduled Monuments are also in close proximity outwith the site and in the wider area, the closest of which are Oliver Castle fort (SM3144) and Oliver Crags fort (SM2947). In addition, there are three Category B Listed Buildings within 1 km of the site.

The site does not lie within any nationally designated landscapes. The Upper Tweeddale National Scenic Area (NSA) covers approximately 12,770 ha and is located approximately 4 km north-west of the site encompassing the expanse between Broughton to Peebles in the north-east. Talla-Hart Fell Wild Land Area (WLA) lies approximately 5 km to the south of the site.

The site lies within the Tweedsmuir Uplands Special Landscape Area (SLA) and the predominant Landscape Character Types (LCTs) of the site are characterised as 'Upland Valley with Pastoral Floor' and 'Southern Uplands Borders'.

## 3.0 Access Baseline

### 3.1 Existing Access within the Site

There are no formal Rights of Way (RoW) or Core Paths through the site as confirmed through consultation with Scottish Rights of Way and Access Society (Scotways) and SBC. However, the site does already include several forestry access tracks which could be utilised by the public for recreation under the right to roam. The 1:25K Ordnance Survey (OS) map shows a path running through the central part of the site connecting Tweedsmuir with the bothy at Hopehead. It has been confirmed through site visits and discussions with the landowner, however, that this path does not exist on the ground and no accessible route is possible through the dense trees planted in this area.

Access to the site would be afforded from the existing forestry access point to the site from the A701, as shown on Figure 3.1.

### 3.2 Access in the Surrounding Area

Consultation with the Scottish Rights of Way and Access Society (Scotways) and the SBC have identified a number of paths including Core Paths and informal routes in the wider area around the site, which are shown on Figure 14.3 and Figure 14.1.1. Figure 14.1.1 provides the paths study requested by SBC in their Scoping consultation showing how existing routes and potential new routes would interact.

There are two RoWs within 5 km to the east of the site, running from Hearthstane to Broad Law and north-south along the Rover Tweed, east of the site. To the north is a Core Path running from Chapelgill Hill to Gathersnow Hill which connects with a wider network of paths within the South Lanarkshire Authority area. To the south of the site is a Core Path leading from the Fruid Road, southwards along the northern shores of the Fruid Reservoir and then south-westwards to join with the Annadale Way (at the Devil's Beeftub) approximately 10 km south of the site. Additionally, the SBC Core Paths Plan (2010) shows two permissive / customary paths (routes with a known history of public use) to the north of the site which provides access along the wind farm tracks of the Glenkerie Wind Farm.

There are several long-distance paths in the surrounding area south of the site in the Scottish Borders and Dumfries and Galloway as well as established routes to popular hill summits. Key routes within the areas include:

- The Southern Upland Way;
- The Annandale Way; and
- The Cross Borders Drove Road.

The River Tweed is located to the south of the site and greater access is being sought from SBC and the community to improve access to and around the River Tweed as part of the wider 'Destination Tweed' project, with the aim of creating a new long-distance route. This route aims to provide a 113 mile trail from Moffat to Berwick-upon-Tweed.

## 4.0 Potential Access Impacts

During the construction period, conflicts with the public may occur when works are taking place on or near existing routes which may be used by the public, including the on-site existing forestry access tracks. The wider RoW and Core Path network would not be affected by construction.

Approximately 3.5 km of existing forestry access tracks on the site have been utilised as part of the Proposed Development layout thereby minimising the amount of new track required to be built for the Proposed Development to approximately 1.5 km.

The infrastructure and traffic requirements for the Proposed Development are outlined within Chapter 12 and Technical Appendix 12.1.

The following elements of the Proposed Development are most likely to affect members of the public wishing to access the site using the existing forestry tracks:

- construction of up to 1.5 km of new on-site access track and 3.5 km of upgraded existing access track with associated drainage, turning heads and passing places;
- clearance of forestry and removal of timber;
- excavation of and construction of turbine foundations (where proposed turbines are proposed alongside existing tracks);
- laying of underground cabling along existing access tracks;
- construction of construction and substation compounds (where these are located adjoining existing tracks);
- formation of and excavation of aggregate from on-site borrow pits;
- access by HGV construction traffic associated with the Proposed Development;
- delivery of turbine components to site; and
- erection of turbines.

In respect of recreational users, disruption from construction works would be temporary and expected to occur only during the construction and decommissioning phases. The routine maintenance related to the operation of the Proposed Development is not expected to cause undue restrictions.

## 5.0 Construction Access Arrangements

### 5.1 Construction Arrangements

During construction, access restrictions and limitations would be kept to a minimum, although it is acknowledged that due to the nature of works needing to upgrade existing forestry tracks, when those works are taking place no public access would be possible. Public access through the site will only be through the eastern gate away from the majority of the construction works and only when construction of the recreational heritage trail and car parking spaces is not happening.

During construction, measures would be required to ensure that the public understand that restricted access to the forestry tracks would be in place throughout the works. Plans for temporary access management, including traffic management and access restrictions, would be communicated with the public prior to taking place, where feasible.

The Applicant is committed to safeguarding the safety of members of the general public, whilst also ensuring that construction progress is not compromised.

The final version of the AMP would include detailed information on how access would be managed by the Applicant. Mitigation measures would need to be implemented prior to, or during construction of the Proposed Development and in outline such measures would include:

- details of the provision of signage and other information alerting the public to construction works;
  - sufficient temporary advance signage would be erected at key access locations both within and outwith the Proposed Development working area prior to the start of construction, to inform all access users of the location and timing of the construction activities;
  - signage (compliant with Traffic Signs Manual Chapter 8 (Dft *et. al* 2009) would also be employed on-site, for both site personnel and the public, to clearly define the boundary of the works and any

crossing points where they coincide with areas accessible to the public. Annex A of this document provides examples of such signs;

- details of any fencing or barriers to be provided during the construction period to ensure separation from construction traffic, including the type of fencing and gate access to be used, where access is not precluded on the basis of health and safety. Access would be maintained for the landowner and forestry management team but locked for members of the public where applicable;
- the Principal Contractor will ensure that speed limits are always adhered to by their drivers and associated subcontractors. Advisory speed limit signage will also be installed on approaches to areas where path users may interact with construction traffic. Signage will be installed on the site exits that makes drivers aware of local speed limits and reminding drivers of the potential presence of pedestrians and cyclists in the area. This will also be emphasised in the weekly toolbox talks;
- details of a communication and liaison strategy (as outlined in Technical Appendix 3.1) to advise local communities and other users of construction works and vehicle movements prior to any construction starting on-site and communication would be maintained on a regular basis until construction is complete and the Proposed Development is operational. This would be aimed at walkers, runners and cyclists;
  - further consultation with stakeholders including SBC's Access Officer. These meetings would be closely aligned with the commencement of construction of the Proposed Development; and
  - the community liaison strategy could be designed not just to establish processes to keep the community informed, but also to review incidents that have occurred and how these have been / should be resolved and to discuss the forthcoming programme of work.

The Applicant is also willing to consider any additional mitigation measures which would reduce the effect of the construction activities on recreational users.

## 5.2 Implementation and Control

The final AMP, together with the final CEMP and CTMP (see Technical Appendices: 3.1 and Chapter 12 for outlines), would be used by the Principal Contractor to ensure that appropriate access and environmental management is implemented throughout the construction phase of the Proposed Development.

The final AMP would set out measures to ensure that recreational users of the site are informed of the construction work and directed into safe areas where there would be no conflict with plant and machinery.

Compliance with the CEMP is the key control measure required during construction to ensure mitigation is appropriately addressed. It documents the principles and processes to be followed to implement all relevant agreed environmental mitigation.

The personnel who would implement, monitor and respond to the CEMP and final AMP would be the Applicant's construction team and the Principal Contractor. The Principal Contractor would be required to prepare a series of method statements in accordance with the CEMP. These method statements would detail how the contractor intends to implement the mitigation set out in the CEMP and would be integrated into detailed Construction Method Statements. The method statements and construction works would be overseen by the Ecological Clerk of Works (ECoW) to ensure compliance.

The Applicant and Principal Contractor would also ensure that during construction of the Proposed Development, the requirements of both the Land Reform Act (Scotland) 2003 (as amended) and the Scottish Outdoor Access Code are met or are exceeded.

## 6.0 Operational Arrangements

During the operational phase of the Proposed Development, all access restrictions and limitations would be removed, and general recreational public access rights under the right to roam would be restored. The final version of the AMP (to be agreed upon) would include detailed information about how access would be managed by the Applicant during the lifespan of the Proposed Development as well as the following aspects:

- details of a communications campaign linked with the end of the construction period; and
- details of the proposed recreational heritage trail which will include details of the path surfacing (which will be partly multifunctional in usage), car parking facilities, signage and interpretation boards including map orientation.

The requirements of the PAMP are to provide infrastructure to support access to the site. This PAMP provides some outline information of the infrastructure proposed in the following sections and should be considered the basis of future discussions between stakeholders.

## 6.1 Wind Farm Access Tracks

Up to 3.5 km of existing forestry tracks will be upgraded and access rights across the site (under the right to roam) will be maintained throughout the operational life of the Proposed Development. Part of the upgraded tracks will form a section of the promoted recreational heritage trail as shown on Figures 3.13 and 14.1.1.

## 6.2 New Paths

To improve access within the site, the Proposed Development would include a recreational heritage trail starting in the south-eastern part of the site and linking into the Proposed Development access tracks and wider forestry tracks, as set out on Figure 3.13.

A stretch of wheelchair-accessible path (approximately 1.2 m wide) (to allow for all abilities' access including wheelchairs and buggies) will be created at the start of the trail approximately 380 m in length, focusing on the interpretation of the two Scheduled Monuments within the site, finishing just before the Hallow Burn.

Two small single-span wooden bridges will be installed over the Hallow Burn (and its tributary) and a new path (approximately 1.2 m wide) (not suitable for wheelchair use or buggies) will be created of locally sourced stone (approximately 620 m) leading to the Proposed Development access tracks to the west.

The recreational heritage trail will provide up to 5 km of walking, running or cycling provision and will be accessed by an existing vehicular access point off the A701 approximately 650 m southwest of Tweedsmuir village. Car parking spaces and bins will be provided.

The recreational heritage trail could form an extension to part of the Destination Tweed project facilitating recreational access around the River Tweed to celebrate and share the nature, history and stories of the River Tweed corridor. Destination Tweed is led by the Tweed Forum and is being developed in collaboration with SBC. It features the creation of a 113-mile trail from Moffat, past the source of the Tweed and on to Berwick upon Tweed where the river meets the sea. This could facilitate the implementation of longer recreational routes and enhance a variety of users' experience of the local and wider area.

## 6.3 Signage and Interpretation Boards

Interpretation boards will be provided at various points along the route to describe environmental features in and around the site focusing on those of heritage and ecological interest. Some examples of the information to be included are shown on Figure 3.13 as follows:

1. A map of the recreational heritage trail and provide information on the two Scheduled Monuments adjacent to the accessible path, cremation cemeteries and prehistoric burial practices in the local area.
2. Provide information on the defence of the Tweed Valley in the prehistoric and later periods and wildlife of interest.
3. Provide information on Prehistoric settlements in the Tweed Valley, focused on the Weird Law platform settlement.
4. Provide information on the Glenmuck Local Biodiversity Site, the formation of peat, why it's important and what the Applicant is doing on site to protect it.
5. Provide information on current uses of the site e.g. commercial forestry and electricity generation.
6. Provide information on work the Applicant is doing for biodiversity enhancement.

The exact information to be provided is subject to agreement, and further interpretation panels could also be designed and installed at points of interest along the trail. These could be identified in cooperation with stakeholders and people with detailed local knowledge providing information on linkages to the wider area.

## 6.4 Management of Vehicular Access

It is anticipated that the Applicant, alongside the landowners and access stakeholders, will be responsible for access management within the site. In order to control vehicular access to the site, and

prevent joy riding or off-road rallying, it would be a necessity to install locked, access barrier gates to restrict non-Proposed Development / forestry related vehicle users, but would still allow for non-vehicular access, as per the Land Reform (Scotland) Act 2003.

## 6.5 Maintenance of Tracks and Paths

As part of the development of the final AMP, a maintenance plan and programme of works would be prepared for the lifetime of the Proposed Development. It is anticipated that route inspections and reasonable maintenance would be implemented by the Applicant.

It is anticipated that maintenance would be carried out on a yearly basis following path and infrastructure inspection. Reasonable maintenance of the recreational heritage trail would be considered and agreed with the Applicant and all other access stakeholders.

## 6.6 Enhancement Timescales

The Applicant has informal agreements with the landowner to deliver the terms of the final AMP but only within the terms of its land agreement, further access proposals would be subject to discussions and agreement with all relevant access stakeholders. The Applicant cannot secure or provide any certainty beyond the 50-year life of the project.

## 7.0 Conclusions

This PAMP is a working document and provides an outline of the range of information, and level of detail that would be provided within the final AMP, should consent be granted for the Proposed Development. It is envisaged that further consultation and discussion with all other access stakeholders, would be necessary.

Through the development of the AMP, the Applicant would consult with other stakeholders including the landowner, SBC Access Officer and the Community Councils/Community Development Trusts.

## 8.0 References

Department for Transport/Highways Agency, Department for Regional Development (Northern Ireland), Transport Scotland and Welsh Assembly Government (2009). *Traffic Safety Measures and Signs for Road Works and Temporary Situations Part 1: Design 2009*. Available at: <https://assets.publishing.service.gov.uk/media/5a74adeaed915d7ab83b5ab2/traffic-signs-manual-chapter-08-part-01.pdf>. Accessed 13 June 2024.

NatureScot (2023), *The Scottish Outdoor Access Code*. Available at: <https://www.outdooraccess-scotland.scot/>. Accessed on 23 May 2024.

Scottish Parliament (2003), *The Land Reform (Scotland) Act 2003*. Available at: <https://www.legislation.gov.uk/asp/2003/2/contents> Accessed on 23 May 2024.

Tweed Forum (2024) Destination Tweed. Available at: <https://tweedforum.org/our-work/projects/destination-tweed/>. Accessed on 23 May 2024.



## ANNEX A

Sample Pedestrian Warning Sign



**Warning!**  
Wind Farm  
Construction Traffic  
Ahead

For further information or in an emergency  
please call: *Site manager, Tel No*

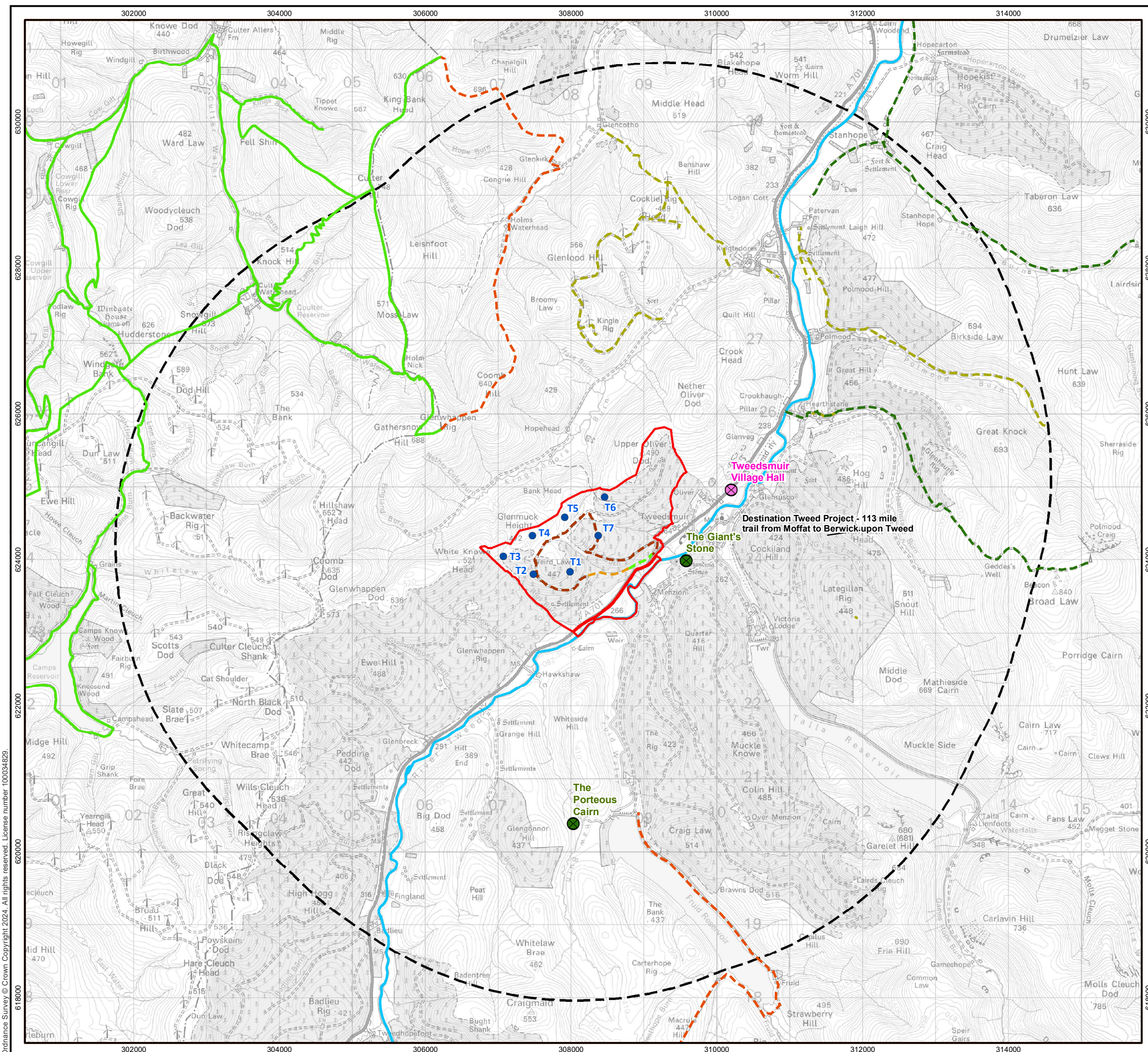
Sample Construction Staff Warning Sign



**Warning!**  
This track is used by the  
public.  
Be vigilant!

For further information or in an emergency  
please call: *Site manager, Tel No*

# FIGURES



- Site Boundary
  - Proposed Turbine Location
  - Local Area of Impact (Site Boundary 5 km Buffer)
  - ⊗ Leisure Receptor
  - Tourism Receptor
  - Wider Network
  - Core Path
  - Right of Way
  - Customary Path
- Proposed Recreational Heritage Trail**
- Existing Track (Not Suitable for Wheelchairs)
  - New Path (Not Suitable for Wheelchairs)
  - New Path (Wheelchair Accessible)
- Recreational Asset**
- River Tweed - Destination Tweed Project

**Note:** Turbine symbols are not to scale



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**Figure 14.1.1**  
**Proposed Paths Plan**  
**Oliver Forest Wind Farm**  
**Environmental Impact Assessment Report**

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