

P e l l F r i s c h m a n n

Oliver Forest Wind Farm

Route Survey Report

June 2024

107077

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1 Introduction

1.1 Purpose of the Report

Pell Frischmann (PF) has been commissioned by Oliver Forest Wind Farm Limited (the Applicant) to undertake a survey of the delivery route for wind turbine Abnormal Indivisible Loads (AIL) associated with the construction and development of Oliver Forest Wind Farm, located in the Tweed Valley in the Southern Uplands of Scotland.

The Route Survey Report (RSR) has been prepared to help inform the Applicant on the likely issues associated with the development of the site with regards to on-site transport and access for AIL traffic. The report identifies the key issues associated with AIL deliveries and notes that remedial works, either in the form of physical works or as traffic management interventions, will be required to accommodate the predicted loads.

The detailed assessment and subsequent designs of any remedial works are beyond the agreed scope of works between PF and the Applicant at this point in time.

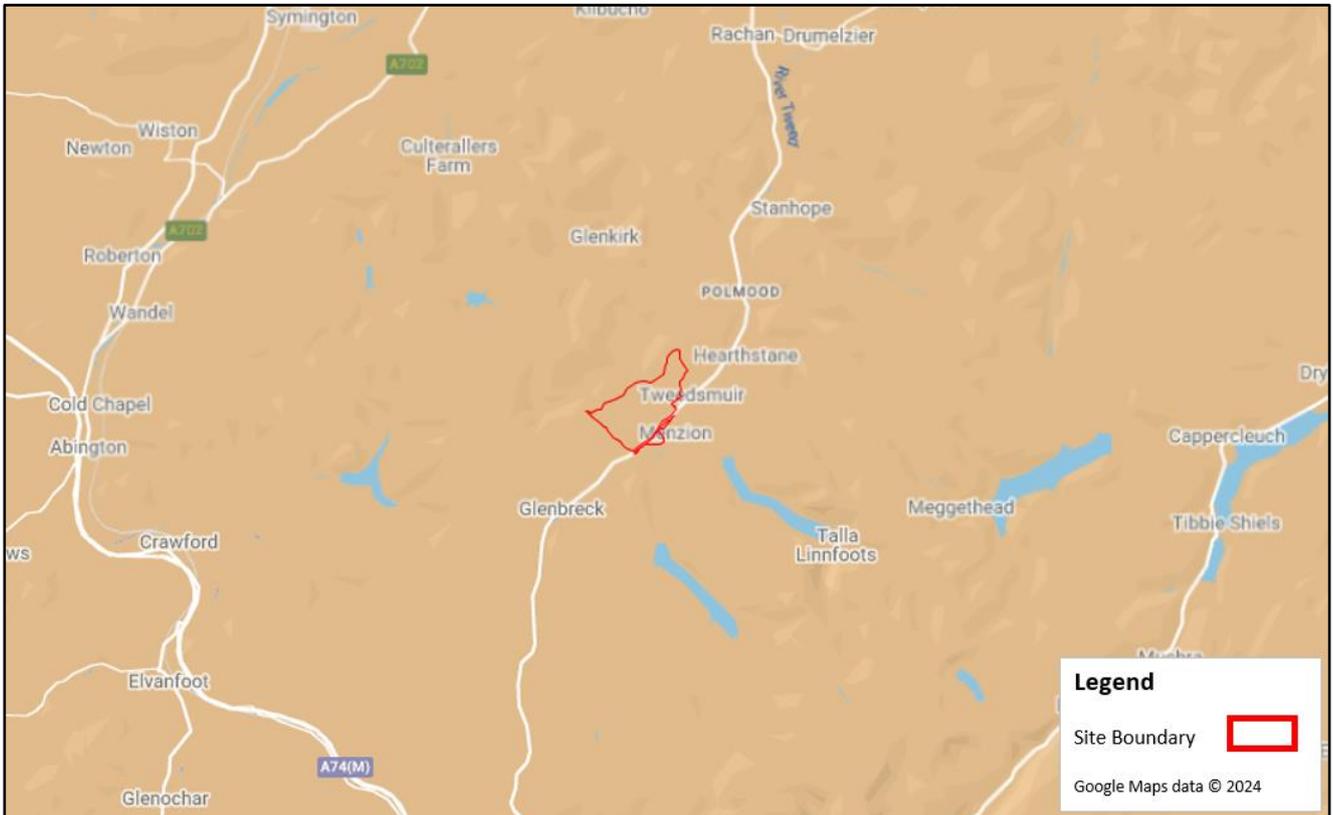
It is the responsibility of the wind turbine supplier to ensure that the entirety of the proposed access route is suitable and meets with their satisfaction. The turbine supplier will be responsible for ensuring that the finalised proposals meet with the appropriate levels of health and safety consideration for all road users and has been made in accordance with the relevant legislation at the time of delivery.

2 Site Background

2.1 Site Location

The Proposed Development site is located in the Tweed Valley in the Southern Uplands of Scotland, spread across the extent of three named hills: Upper Oliver Dod, Weird Law, and Glenmuck Height. Figure 2-1 illustrates the general site location.

Figure 2-1: Site Location Plan



2.2 Candidate Turbines

The Applicant has indicated that they wish to consider the use of a Nordex N163 turbine at a tip height of 200m.

The details of the components are summarised in Table 2-1.

Table 2-1: N163 Turbine Size Summary

Component	Length (m)	Width (m)	Height (m)	Weight (t)
Blade	81.5	4.395	4.110	28.871
Base Tower	11.561	4.300	4.292	83.318
Mid Tower 1	16.430	4.292	4.286	82.391
Mid Tower 2	21.125	4.286	4.279	81.401
Mid Tower 3	29.972	4.279	4.268	82.563
Top Tower	35.000	4.268	3.258	60.569

2.3 Proposed Delivery Equipment

To provide a robust assessment scenario based upon the known issues along the access route, it has been assumed that all blades would be carried on a Super Wing Carrier trailer.

Towers would be carried in a 4+7 clamp adaptor style trailer, whereas loads such as the hub, nacelle housing, and top towers would be carried on a six-axle step frame trailer.

Figure 2-2: Super Wing Carrier Trailer



Figure 2-3: Tower Trailer



3 Access Route Review

3.1 Port of Entry

The proposed Port of Entry (POE) is KGV Docks in Glasgow. The port is the closest suitable port to site and as such is in line with the Government's "Water Preferred" policy¹ towards AIL movements.

The port has been used by renewables deliveries in the past for a number of wind farms, including Kype Muir, Kilgalloch, and Clyde wind farms.

The port has sufficient quay and storage space and is well located for the strategic trunk road network.

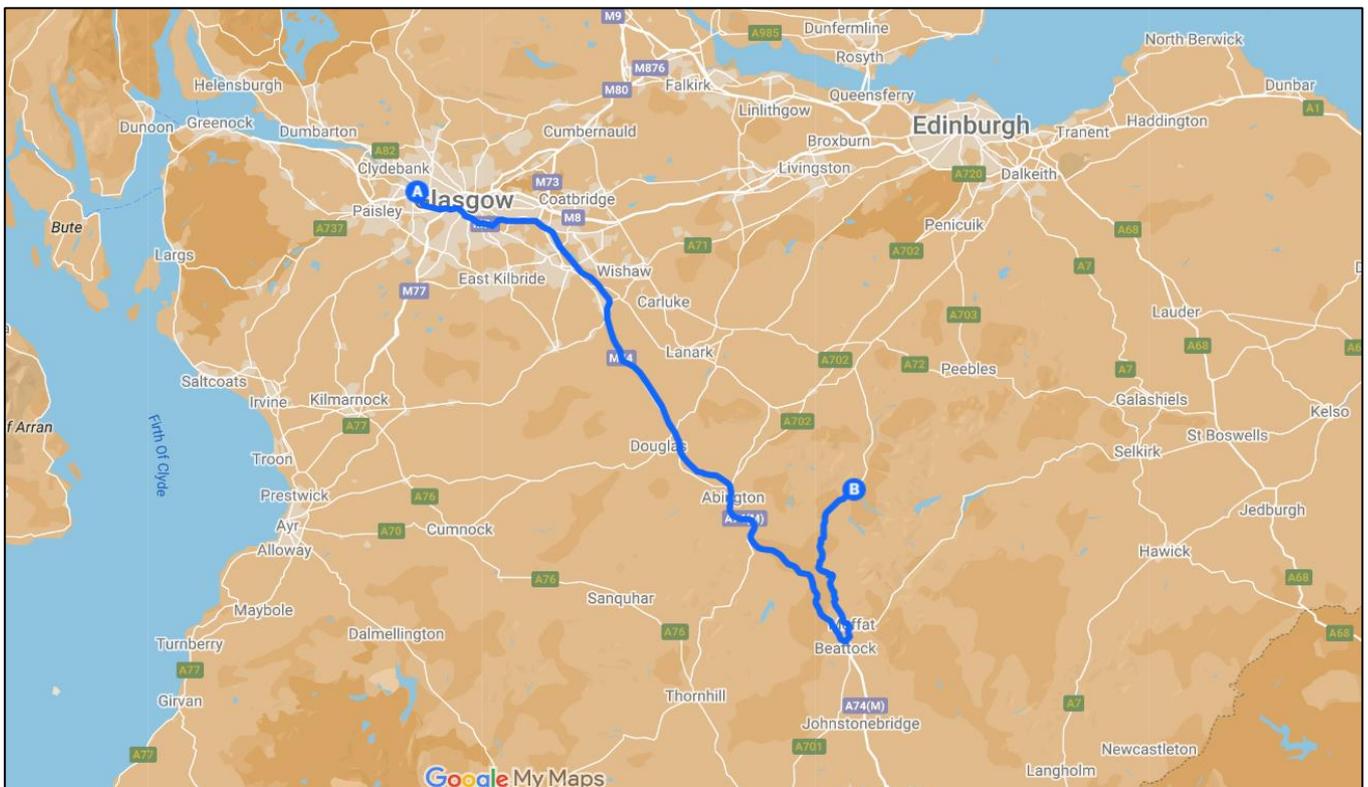
3.2 Proposed Access Route

The proposed access route from KGV Docks to the site access junction is as follows:

- Loads would exit KGV Docks in Glasgow onto Kings Inch Drive;
- Loads would continue along Kings Inch Drive before turning left onto the M8 slip road, Mayo Avenue;
- Loads would then merge onto the M8 at junction 25A;
- Vehicles would continue east on the M8 to Junction 21 where they would join the M74 travelling south;
- Loads would depart the A74(M) at Junction 15;
- Loads would turn left at the Junction 15 roundabout and would join the A701 northbound, passing through Moffat;
- Vehicles would continue on the A701 northbound;
- To the south of Tweedsmuir, loads would turn left into an upgraded access junction and would continue to site using private access tracks.

The proposed access route is illustrated in Figure 3-1.

Figure 3-1: Proposed Access Route



¹ Highways England (2019), Water Preferred Policy – Guidelines for the movement of abnormal indivisible loads

3.3 Route Constraints

The constraints noted on the route are detailed in Table 3-1. No consideration of the transport issues within the Proposed Development site have been undertaken.

Plans illustrating the location of the constraints are provided in Appendix A.

Table 3-1: Constraint Points and Details

POI	Key Constraint	Details
1	Kings Inch Drive Roundabout 1 	<p>Loads will exit the port driving through the roundabout onto Kings Inch Drive heading northwest.</p> <p>Loads will overrun and oversail the central island of the roundabout where the existing overrun area should be utilised.</p> <p>Loads will also oversail the central reservation of the exit arm of the roundabout where two road signs should be removed.</p> <p>Loads will oversail the western verge of the exit arm.</p> <p>Swept path drawing SK01 is included in Appendix B.</p>
2	Kings Inch Drive Roundabout 2 	<p>Loads will proceed ahead at the roundabout, taking the second exit.</p> <p>Loads will oversail both verges of the entry arm on approach. One lighting column should be removed from the southern verge.</p> <p>Loads will oversail the southern verge of the roundabout island where two road signs to be removed.</p> <p>Vehicles will oversail the southern verge of the exit arm.</p> <p>Swept path drawing SK02 is included in Appendix B.</p>
3	Kings Inch Drive Roundabout 3 	<p>Loads will take the second exit at the roundabout.</p> <p>Loads will oversail the southern verge of the roundabout where a load bearing surface should be laid.</p> <p>Swept path drawing SK03 is included in Appendix B.</p>
4	Kings Inch Drive / Mayo Avenue Junction 	<p>Loads will turn left at the junction joining the M8 slip road.</p> <p>Loads will oversail the central reserve on approach where escorts are required to hold oncoming vehicles during movements. One lighting column, one traffic signal and a section of pedestrian guardrail should be removed. Loads will oversail the northern footway of the entry arm.</p> <p>Loads will oversail the splitter island where three traffic signals, one pedestrian call post and a section of guardrail should be removed. One bollard will be oversailed.</p> <p>Loads will oversail the inside of the junction where one pedestrian call post and one road sign should be removed. Trees and vegetation should be trimmed.</p> <p>Swept path drawing SK04 is included in Appendix B.</p>

POI	Key Constraint	Details
5	<p>Mayo Avenue Bend Leading to M8 Junction 25A</p> 	<p>Loads will pass under the A8 on the slip road before joining the M8 headed southeast.</p> <p>After passing the bridge loads will oversail a safety barrier on the left-hand side of the road.</p> <p>Swept path drawing SK05 is included in Appendix B.</p>
6	<p>A74(M) Junction 15 / A701 Roundabout</p> 	<p>Loads will depart the A74(M) at Junction 15 and join the A701 heading northeast. When exiting the A74(M) via the slip lane and approaching Junction 15, escorts should ensure that trailing traffic does not try to merge into the convoy.</p> <p>The blade tip will oversail the right-hand verge of the entry arm where one lighting column, one road sign and fence should be removed. A section of safety barrier and one junction box will be oversailed. Trees and vegetation should be cleared. Land reprofiling may be required to allow the blade tip to oversail; therefore, it is highly recommended that the swept path assessment is repeated on a topographical base survey to confirm the required mitigation.</p> <p>Loads will overrun and oversail the northern verge of the entry arm where a load bearing surface should be laid. One road sign, one lighting column and a section of fence should be removed. Trees and vegetation should be removed. Land should be reprofiled to allow oversail.</p> <p>Vehicles will overrun and oversail the northern verge of the roundabout island where a load bearing surface should be laid and one set of chevron signs should be removed.</p> <p>Vehicles will oversail the exit splitter island where two bollards should be removed. The vehicles will overrun and oversail the eastern verge of the exit arm where a load bearing surface should be laid.</p> <p>Swept path drawing SK06 is included in Appendix B.</p>
7	<p>A701 Entering Moffat</p> 	<p>Loads will continue north through two bends towards Moffat.</p> <p>Loads will oversail both verges through the section. There is minimal clearance to structures to the east of the road and it is recommended that a topographical survey is utilised to confirm the feasibility of the manoeuvre.</p> <p>Loads will overrun and oversail the western verge through the second bend where a load bearing surface should be laid. One utility pole should be removed and vegetation should be trimmed.</p> <p>Swept path drawing SK07 is included in Appendix B.</p>

POI	Key Constraint	Details
8	<p>A701 Entering Moffat</p> 	<p>Loads will continue north on the A701 on approach to Moffat.</p> <p>Loads will oversail both verges through a right bend. Trees and vegetation on the eastern verge should be trimmed. On the western verge, one lighting column, one road sign and one sign post should be removed.</p> <p>Traffic calming measures have been put in place shortly after the bend by way of two splitter islands with associated signage. Load bearing surfaces should be laid and four bollards should be removed.</p> <p>Swept path drawing SK08 is included in Appendix B.</p>
9	<p>A701 Moffat</p> 	<p>Loads will continue north through the Moffat town centre. A traffic management plan will be required to control the movement of vehicles through the town centre.</p> <p>Loads will oversail the western verge of the road at two locations where parking should be suspended.</p> <p>Loads will oversail one splitter island where one lighting column, two road signs and one camera post should be removed.</p> <p>Swept path drawing SK09 is included in Appendix B.</p>

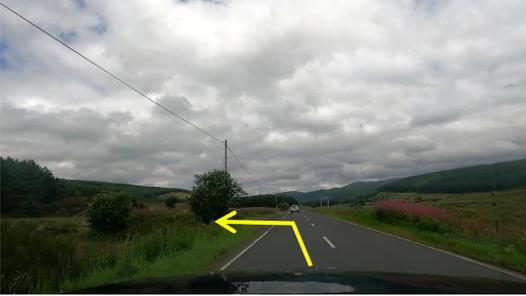
POI	Key Constraint	Details
10	<p>A701 High Street and Academy Road Junction</p> 	<p>Loads will remain on the A701 exiting Moffat through a left bend with a mini roundabout. Loads will keep left, taking the first exit at the roundabout.</p> <p>On approach to the bend loads will oversail both verges of Harthope Place, an eastbound side road, where one lighting column, two sign posts, two decorative planters and a section of pedestrian guardrail should be removed.</p> <p>On the southwestern verge through the bend, loads will oversail the footway where one road sign should be removed.</p> <p>Loads will overrun and oversail the northeastern verge through the bend as they approach the roundabout. A load bearing surface should be laid. One lighting column, one junction box, a section of pedestrian guardrail and vegetation should be removed.</p> <p>At the mini roundabout, loads will overrun and oversail the splitter islands of all three arms where load bearing surfaces should be laid and five bollards should be removed.</p> <p>Loads will overrun and oversail the northern verge of the exit arm where a load bearing surface should be laid. One lighting column and one road sign should be removed.</p> <p>Swept path drawing SK10 is included in Appendix B.</p>
11	<p>A701 South of Moffat Cemetery</p> 	<p>Loads will head northwest on the A701 towards Moffat Cemetery through a right bend.</p> <p>Loads will oversail the northern verge of the A701 through the bend.</p> <p>Two traffic islands should be cleared of street furniture. Throughout the route, the tree canopy needs to be trimmed to provide a clear 5m head height. Trimming of the tree canopy can be subject to ecological constraints and it is suggested that early consultation with the local road authority is undertaken to agree cutting times and permits.</p> <p>Swept path drawing SK11 is included in Appendix B.</p>
12	<p>A701 Moffat Cemetery</p> 	<p>Loads will drive past Moffat Cemetery through a series of bends.</p> <p>Approaching the first bend, the blade tip will oversail the southern verge of the A701 where trees and vegetation should be removed. Loads will overrun and oversail the eastern verge where a load bearing surface should be laid. One utility pole should be removed. One bollard and a section of fence will be oversailed. Third party land is required.</p> <p>Loads will oversail a series of bollards on the western verge through the second and third bend. One gate and fence should be removed. Trees and vegetation should be cleared. Third party land is required.</p> <p>Loads will oversail a series of bollards on the eastern verge through the third bend. Trees and vegetation should be cleared. Third party land is required.</p> <p>Swept path drawing SK12 is included in Appendix B.</p>

POI	Key Constraint	Details
13	A701 North of Moffat Cemetery 	<p>Loads will continue north on the A701.</p> <p>Loads will oversail both verges. On the western verge, a series of bollards will be oversailed and vegetation should be trimmed at two locations. On the eastern verge, one road sign and tree should be removed.</p> <p>Swept path drawing SK13 is included in Appendix B.</p>
14	A701 Bends South of Adamsholm 	<p>Loads will continue on the A701 northbound through two bends.</p> <p>Loads will oversail both verges through both bends. On the western verge, a wall section should be removed from each bend where there is oversail. Trees and vegetation should be cleared. Land reprofiling is required at both bends to allow the blade tip to oversail. On the eastern verge, vegetation should be trimmed.</p> <p>Swept path assessment SK14 is included in Appendix B.</p>
15	A701 Bends at Adamsholm 	<p>Loads will remain on the A701 heading north through two bends.</p> <p>Loads will oversail a series of bollards on the eastern verge through the first bend where three sets of chevron signs should be removed. Trees and vegetation should be cleared.</p> <p>Loads will oversail the western verge through both bends. Trees and vegetation should be cleared.</p> <p>Loads will oversail the eastern verge through the second bend. One bollard will be oversailed. Trees and vegetation should be cleared.</p> <p>Throughout the route, the tree canopy needs to be trimmed to provide a clear 5m head height. Trimming of the tree canopy can be subject to ecological constraints and it is suggested that early consultation with the local road authority is undertaken to agree cutting times and permits.</p> <p>Swept path drawing SK15 is included in Appendix B.</p>
16	A701/B719 Junction 	<p>Loads will remain on the A701 heading north.</p> <p>Loads will oversail the eastern verge passing through the junction. One road sign should be removed and vegetation should be trimmed.</p> <p>Swept path drawing SK16 is included in Appendix B.</p>

POI	Key Constraint	Details
17	<p>A701 Holehouse Bridge and Bends</p> 	<p>Loads will continue north through a right/left bend section.</p> <p><i>Indicative road edges have been provided for illustration only due to the lack of detail on available OS base mapping. Mitigation measures should be confirmed through the test run.</i></p> <p>Loads will oversail both verges through the initial right bend where third party land is required on both sides of the road. The loads should be raised using vehicle suspension to enable oversail of the bridge parapet and safety barrier on the inside of the bend where trees and vegetation should be cleared. Loads will oversail the bridge parapet and safety barrier on the outside of the bend where two sets of chevron signs should be removed and trees and vegetation should be removed.</p> <p>Loads will oversail both sides of the left bend section. On the outside of the left bend, one road sign should be removed and a safety barrier will be oversailed. On the inside, a series of bollards will be oversailed and vegetation should be trimmed.</p> <p>Swept path assessment SK17 is included in Appendix B.</p>
18	<p>A701 West of Granton</p> 	<p>Loads will remain on the A701 heading northwest through a left bend.</p> <p>Loads will oversail both verges. On the left verge, vegetation should be trimmed.</p> <p>Swept path drawing SK18 is included in Appendix B.</p>
19	<p>A701 East of Archie's Hill</p> 	<p>Loads will remain on the A701 heading north through a right bend where loads will oversail the eastern verge.</p>
20, 21	<p>A701 Auldhousehill Bridge</p> 	<p>Loads will continue northbound on the A701 through two right bends.</p> <p>Through the first bend, loads should be raised using vehicle suspension to allow oversail of the safety barrier on the western verge and the fence on the eastern verge. Vegetation should be cleared from both sides of the bend. On the western verge, one set of chevron signs should be removed.</p> <p>Through the second bend, loads will oversail both verges. On the western verge, vegetation should be cleared. On the eastern verge vegetation should be trimmed.</p> <p>Swept path assessment SK19 is included in Appendix B.</p>

POI	Key Constraint	Details
<p>22-25</p>	<p>A701 Ericstane Bends</p> 	<p>Loads will continue north through a series of bends on the A701.</p> <p><i>The OS mapping does not show the road edge. An indicative road edge has been provided for illustration only and mitigation should be confirmed on a topographical base survey or during the test run.</i></p> <p>The swept path assessment indicates that loads will oversail both verges throughout the section.</p> <p>On the left-hand side of the road, trees and vegetation should be cleared. Land reprofiling is required to allow the blade tip to oversail at several locations. Through the final bend, one utility pole and fence should be removed and a series of bollards will be oversailed. Third party land is required.</p> <p>On the right-hand side of the road, three sets of chevron signs and one wooden signpost should be removed. The safety barrier will be oversailed through the first three bends and a fence will be oversailed through the final bend. Vegetation should be cleared through the first three bends. Third party land is required.</p> <p>Swept path assessment SK20 is included in Appendix B.</p>
<p>26</p>	<p>A701 Old Road Bends</p> 	<p>Loads will continue north through two right bends.</p> <p>Loads will oversail both verges through the bends. On the western verge, three utility poles should be removed. On both verges, vegetation should be cleared.</p> <p>Swept path assessment SK21 is included in Appendix B.</p>

POI	Key Constraint	Details
27	<p>A701 Smid Hope Burn</p> 	<p>Loads will continue north on the A701 through a right bend.</p> <p>Loads will oversail both verges through the bend.</p> <p>On the eastern verge, trees and vegetation should be cleared.</p> <p>On the western verge, one bollard and a section of safety barrier will be oversailed. Vegetation should be cleared.</p> <p>Swept path drawing SK22 is included in Appendix B.</p>
28	<p>A701 Smid Hope Burn</p> 	<p>Loads will continue north on the A701 through a left bend.</p> <p>Loads will oversail both verges through the bend. On the right-hand verge, trees should be trimmed. On both verges, vegetation should be trimmed.</p> <p>Swept path drawing SK23 is included in Appendix B.</p>
29	<p>A701 Fingland</p> 	<p>Loads will continue north on the A701 through a right bend.</p> <p>Loads will oversail both verges through the bend. On the western verge, one tree should be removed. On the eastern verge two bollards should be removed. Vegetation should be trimmed.</p> <p>The vertical profile of the road at this location is pronounced and should be reviewed during the test run stage to ascertain if tar wedges will be required to prevent grounding.</p> <p>Swept path drawing SK24 is included in Appendix B.</p>
30	<p>A701 East of Risingclaw Heights</p> 	<p>Loads will continue north at the junction.</p> <p>Vehicles will oversail the western verge on approach to the junction where vegetation should be trimmed.</p> <p>A series of bollards on the eastern verge of the junction should be removed. Vegetation should be cleared.</p> <p>Upon leaving the junction, a series of bollards in the western verge should be removed.</p> <p>Swept path drawing SK25 is included in Appendix B.</p>
31	<p>A701 West of Menzion Bend</p> 	<p>Loads will cross the bridge heading east on the A701.</p> <p><i>The OS mapping does not show the road edge. An indicative road edge has been provided for illustration only and mitigation should be confirmed on a topographical base survey or during the test run.</i></p> <p>Loads will oversail the southern verge of the A701 through this section. The bridge parapet will be oversailed and one utility pole should be removed.</p> <p>Swept path drawing SK26 is included in Appendix B.</p>

POI	Key Constraint	Details
32	Proposed Access Junction 	<p>Loads will turn left at the proposed site access junction.</p> <p>The junction should be upgraded to meet turbine manufacturer and road authority standards. Detailed design is required. Third party land is required.</p> <p>Tree canopy trimming and relocating telephone lines in advance of the junction will be required</p>

3.4 Swept Path Assessment Results and Summary

The detailed swept path drawings for the locations assessed are provided in Appendix B and illustrate tracking undertaken for the worst-case loads.

The colours illustrated on the swept paths are:

- Grey / Black – OS / Topographical Base Mapping;
- Green – Vehicle body outline (body swept path);
- Red – Tracked pathway of the wheels (wheel swept path); and
- Purple – The over-sail tracked path of the load where it encroaches out with the trailer (load swept path).

Where mitigation works are required, the extents of over-run and over-sail areas are illustrated on the swept path drawings.

Please note that where assessments have been undertaken using Ordnance Survey (OS) base mapping, there can be errors in the data source. The available OS mapping does not identify the road edge through some sections. An indicative road edge has been provided for illustration only and should be confirmed through a test run or the completion of a topographical survey at these locations.

Where provided by the client, topographical data has been utilised. Please note that PF cannot accept liability for errors on the data source, be that OS base mapping or client supplied data.

3.5 Land Ownership

The limits of road adoption can vary depending upon the location of the site and the history of the roads agency. The adopted area is generally defined as land contained within a defined boundary where the road agency holds the maintenance rights for the land from the original landowner. In urban areas, this is usually defined as the area from the edge of the footway across the road to the opposing footway back edge.

In rural areas the area of adoption can be open to greater interpretation as defined boundaries may not be readily visible. In these locations, the general rule is that the area of adoption is between established fence / hedge lines or a maximum 2m from the road edge. This can vary between areas and location.

3.6 Summary Issues

It is strongly suggested that following a review of the RSR, the Applicant should undertake the following prior to the delivery of the first abnormal loads, to ensure load and road user safety:

- That any necessary topographical surveys are undertaken, and the swept path results repeated to assist in land negotiations, detailed design works, and permitting;
- A revised review of axle loading on structures along the entire access route with the various road agencies is undertaken immediately prior to the loads being transported in case of last-minute changes to structures;
- A review of clear heights with utility providers and the transport agencies along the route to ensure that there is sufficient space to allow for loads plus sufficient flashover protection (to electrical installations);
- That any verge vegetation and tree canopies which may foul loads is trimmed prior to loads moving;
- That a review of potential roadworks and or closures is undertaken once the delivery schedule is established in draft form;
- That a test run is completed to confirm the route and review any vertical clearance issues; and
- That a condition survey is undertaken to ascertain the extents of road defects prior to loads commencing to protect from spurious damage claims.

4 Summary

4.1 Summary of Access Review

PF has been commissioned by Oliver Forest Wind Farm Limited (the Applicant) to prepare a Route Survey Report to examine the issues associated with the transport of ALL turbine components associated with the construction and development of Oliver Forest Wind Farm, located in the Tweed Valley in the Southern Uplands of Scotland.

This report identifies the key points and issues associated with the proposed route and outlines the issues that will need to be considered for successful delivery of components.

The report is presented to the Applicant for consideration. Various road modifications and interventions are required to successfully access the site. If these are undertaken, access to the consented wind farm site is considered feasible.

4.2 Further Actions

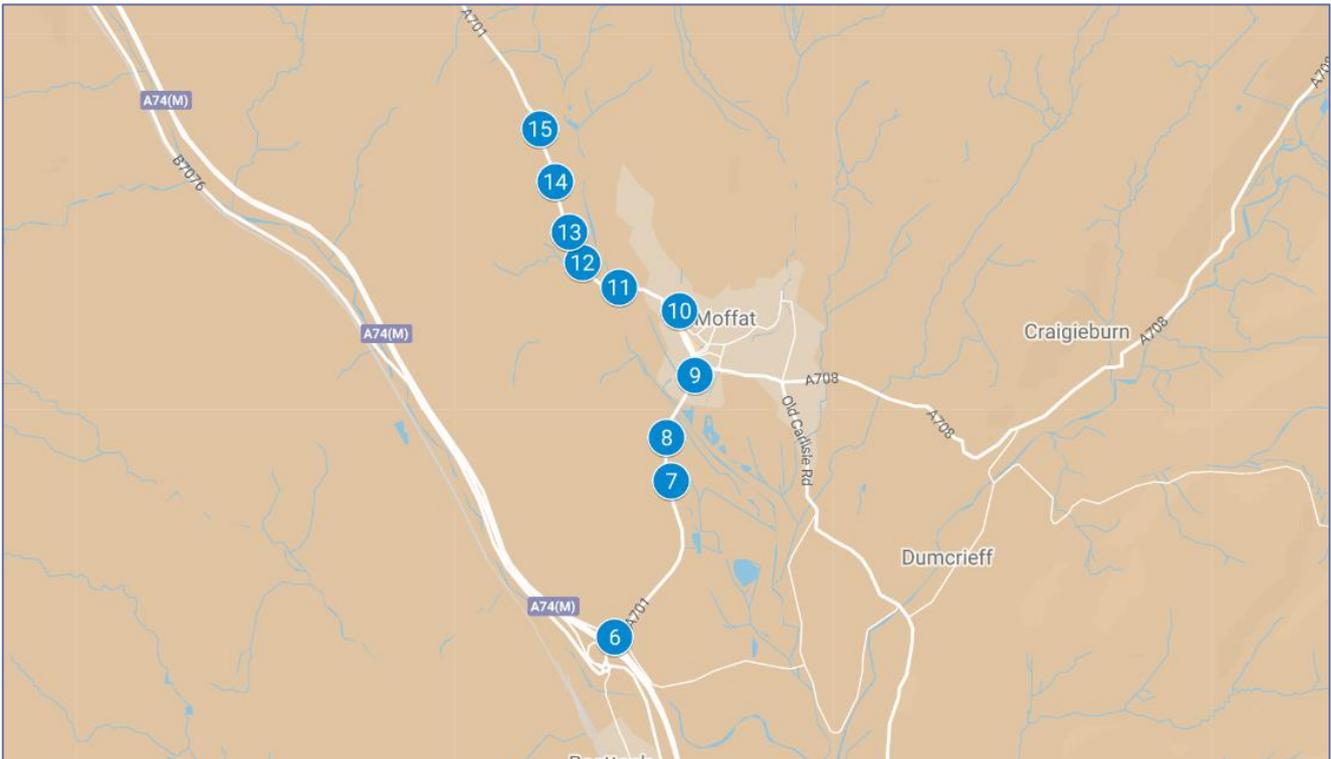
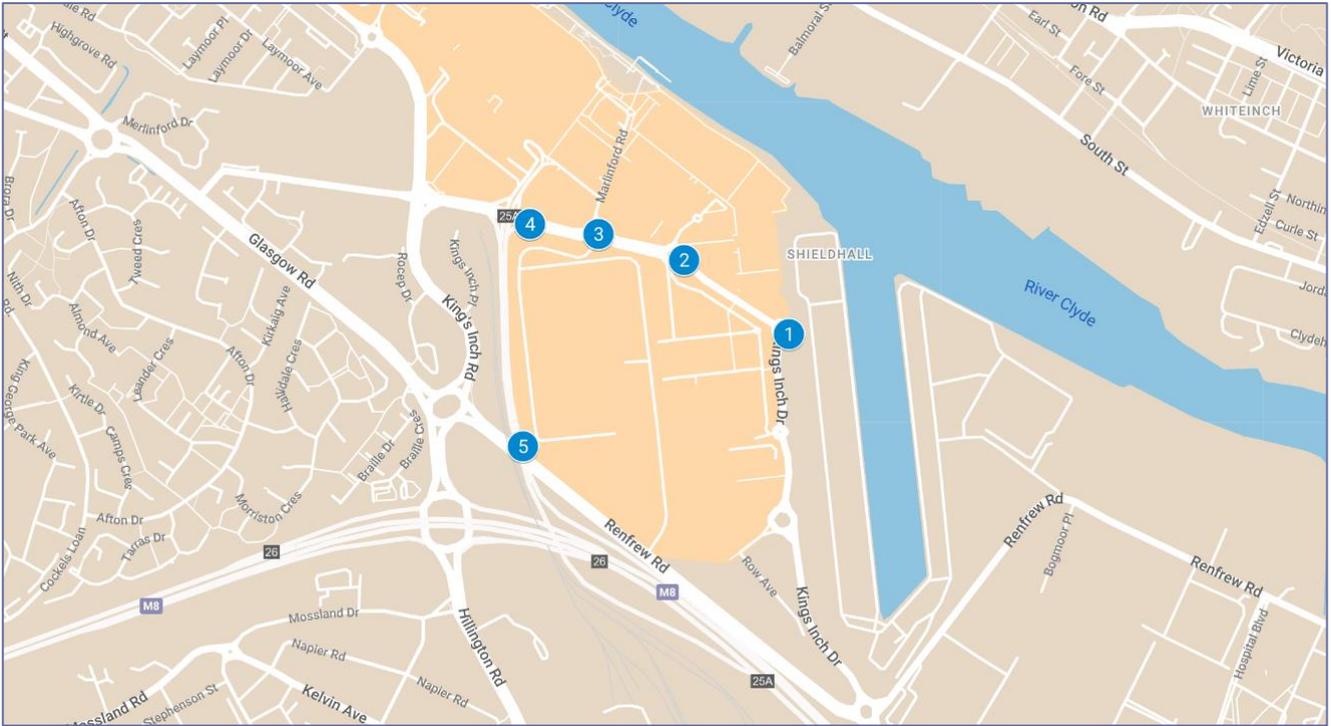
The following actions are recommended to pursue the transport and access issues further:

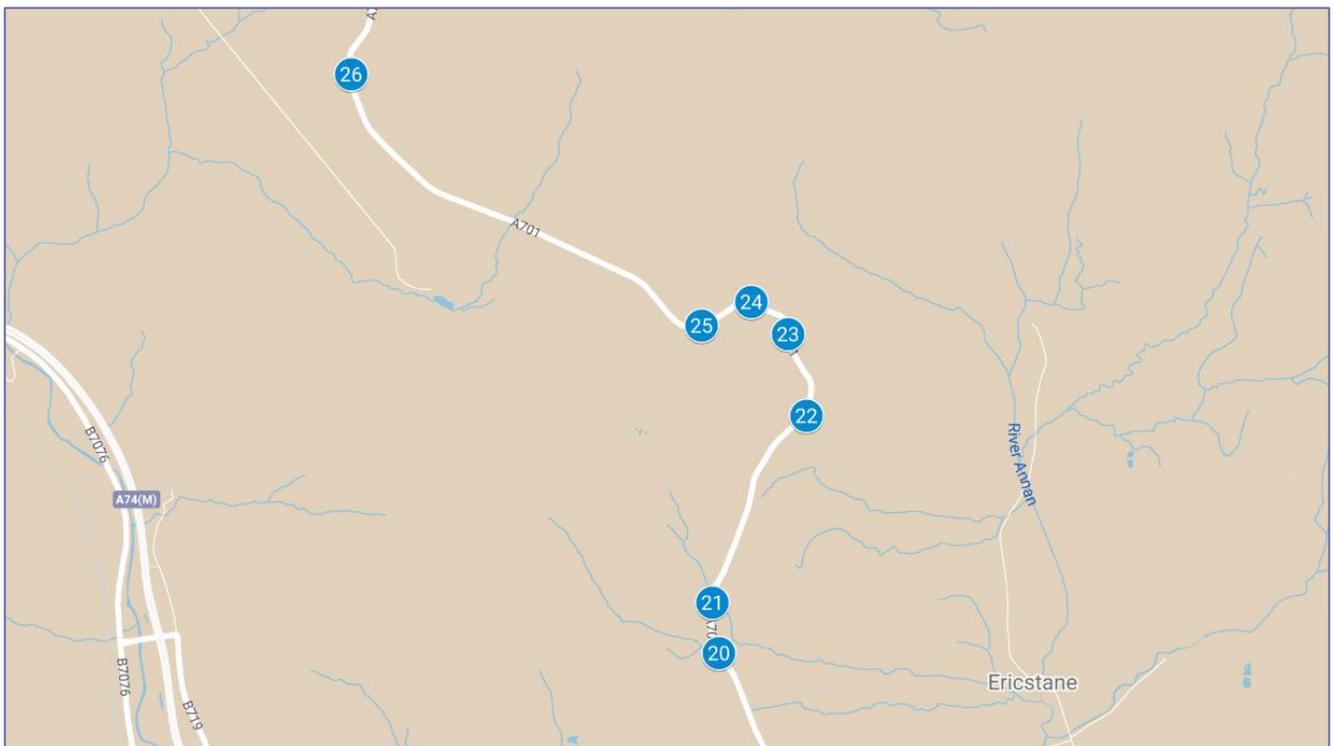
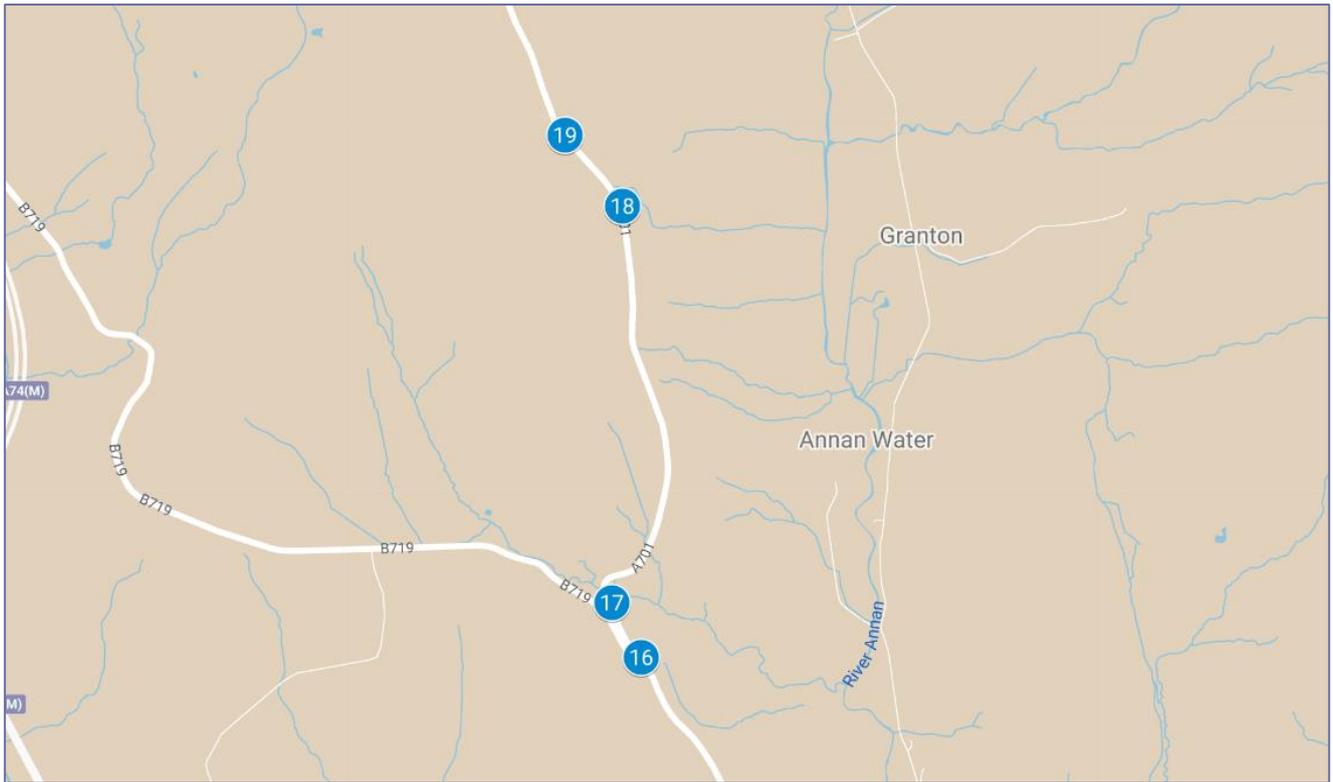
- Undertake detailed design for the proposed new track at the access junction;
- Obtain landowner agreements where third party land is required;
- Undertake the required topographical surveys and repeat the swept path assessments where required;
- Prepare detailed mitigation design proposals to help inform the land option / consultee discussions;
- Obtain the necessary land options (if required);
- Undertake discussion with the affected utility providers and roads agencies;
- Obtain the necessary statutory licences to enable the mitigation measures; and
- Develop a detailed operational Transport Management plan to assist in transporting the proposed loads.

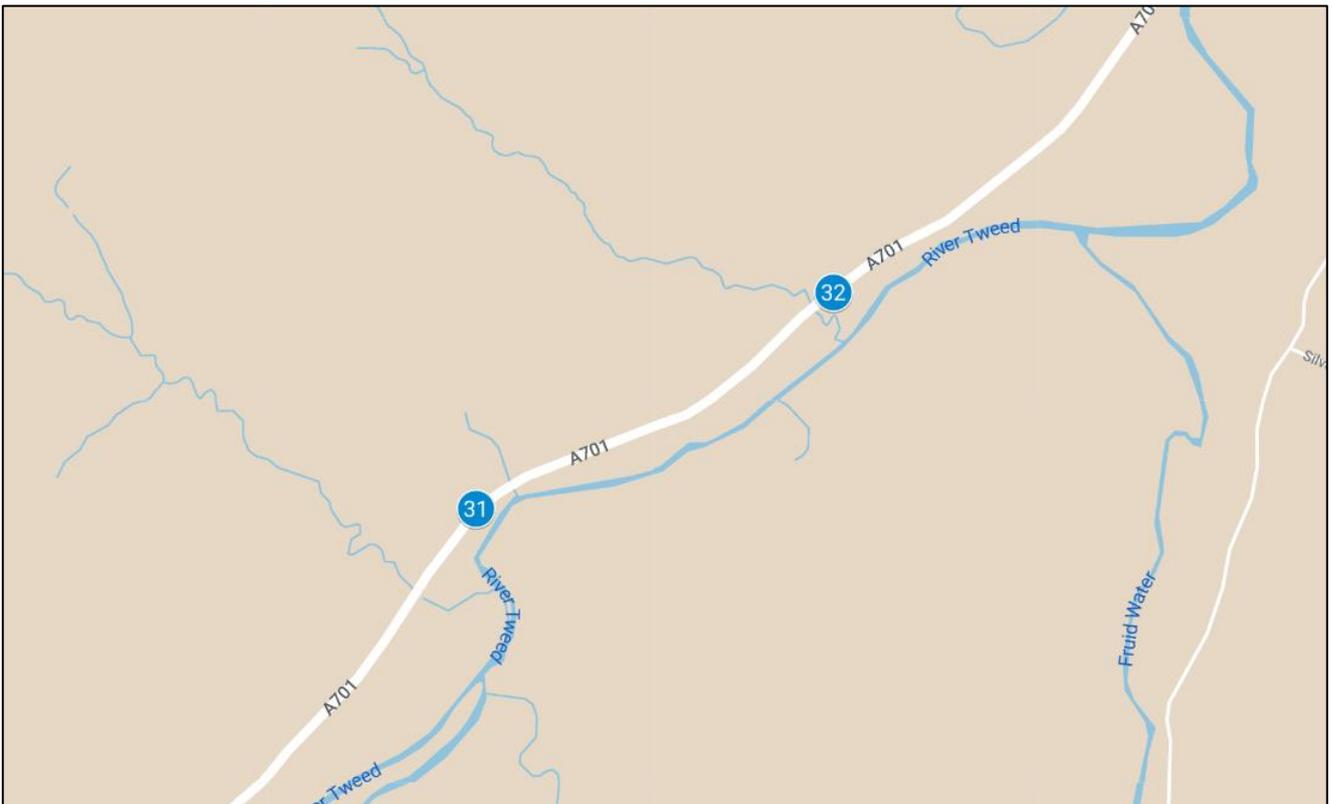
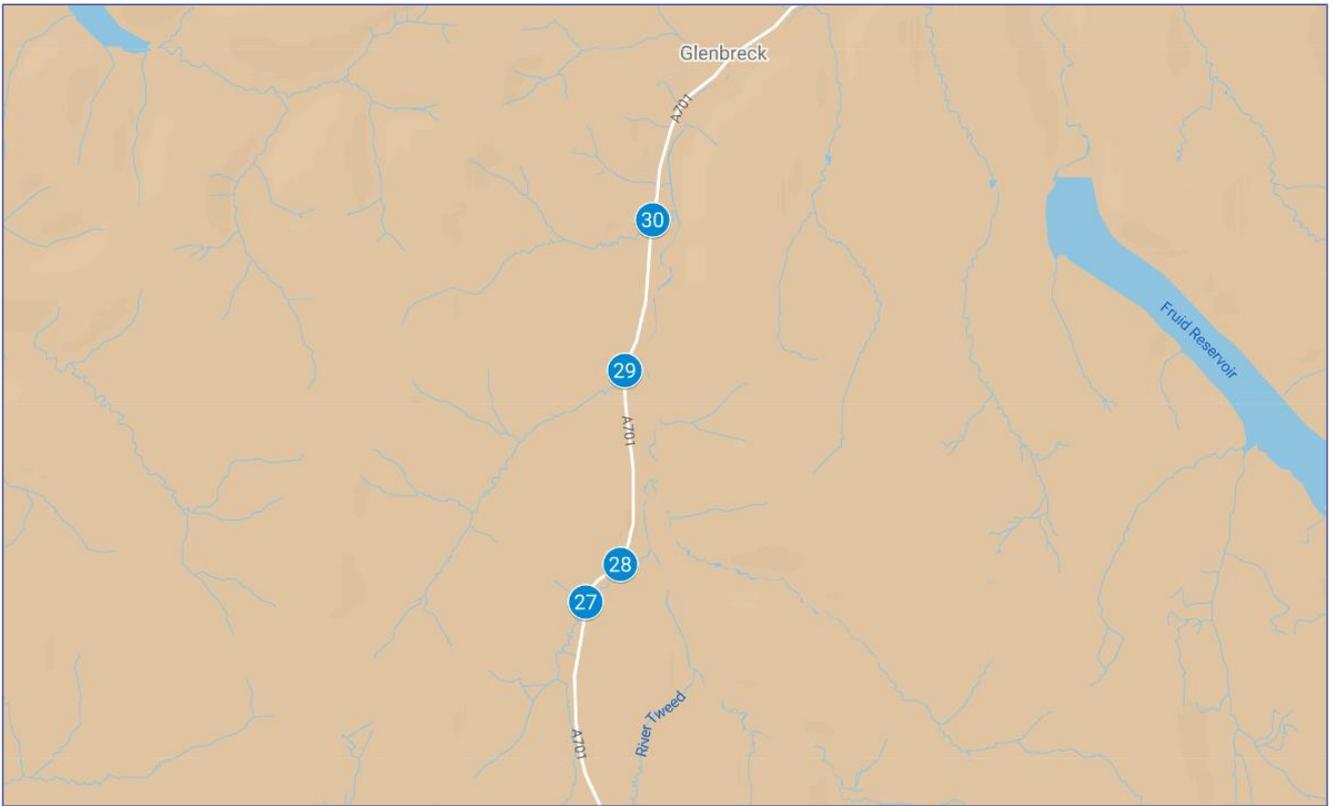
Appendix A Points of Interest Locations

An electronic version of the POI plan can be found here:

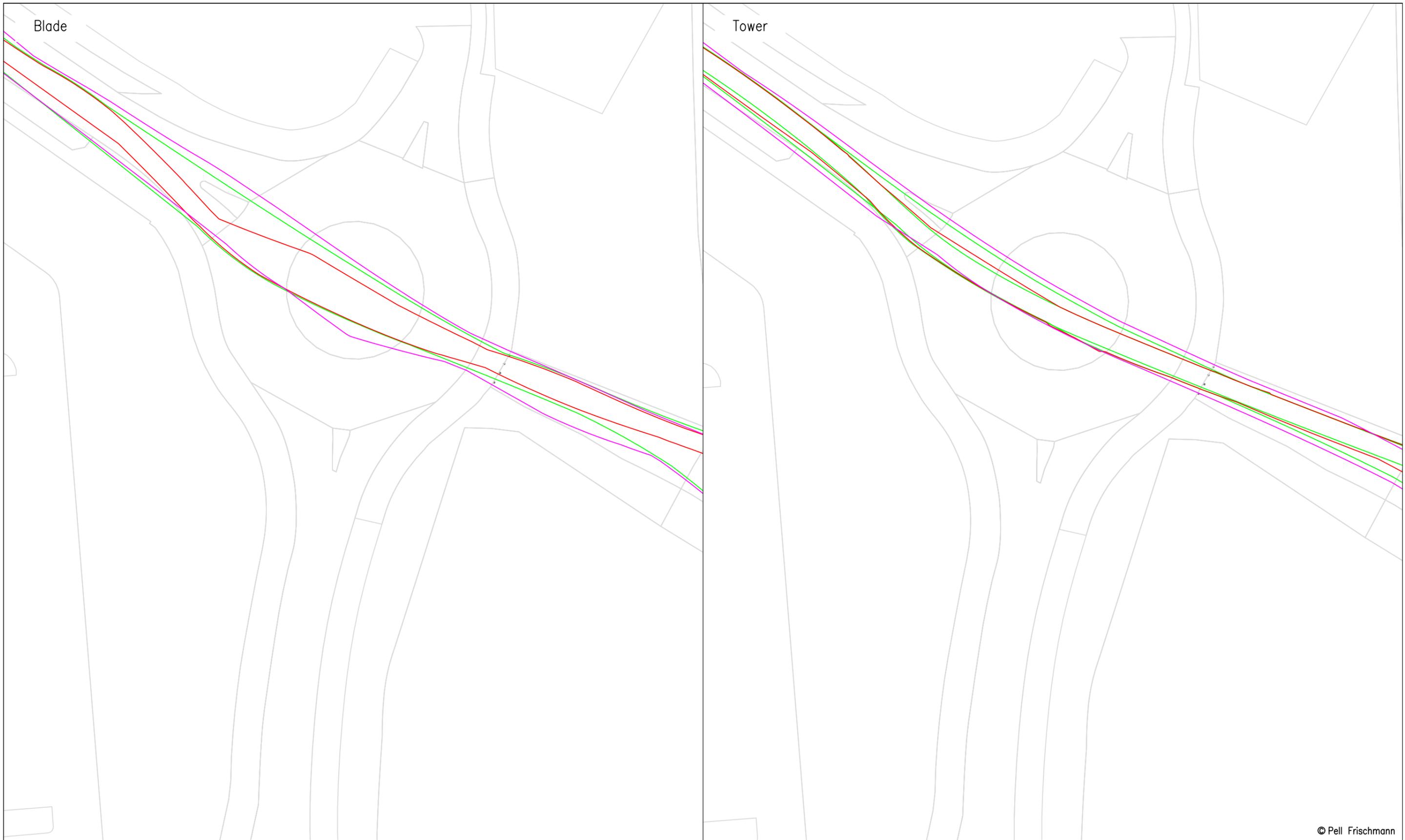
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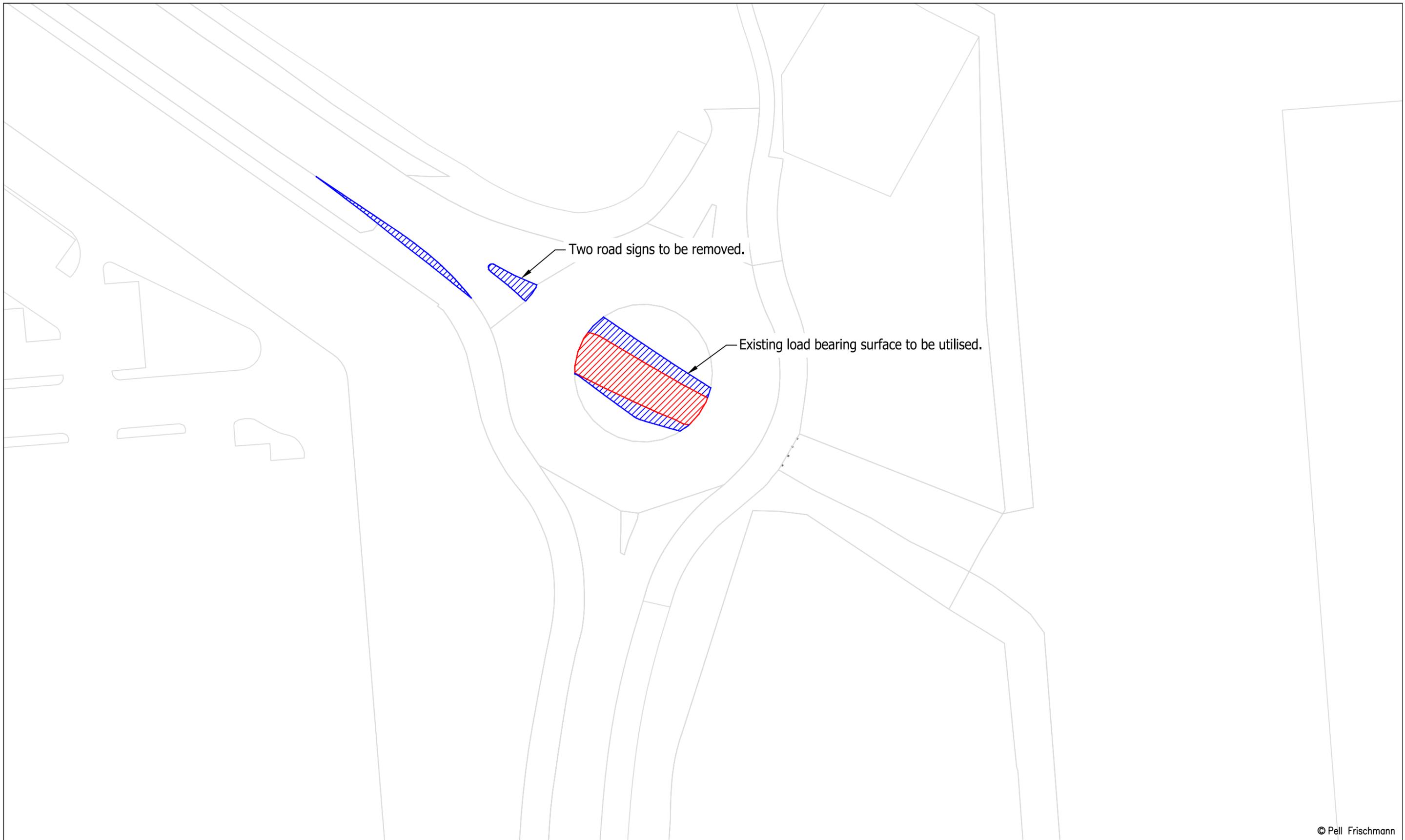


Appendix B Swept Path Assessments



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	Kings Inch Drive Roundabout 1	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest	1		Drawing No.	SK01	Notes:
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

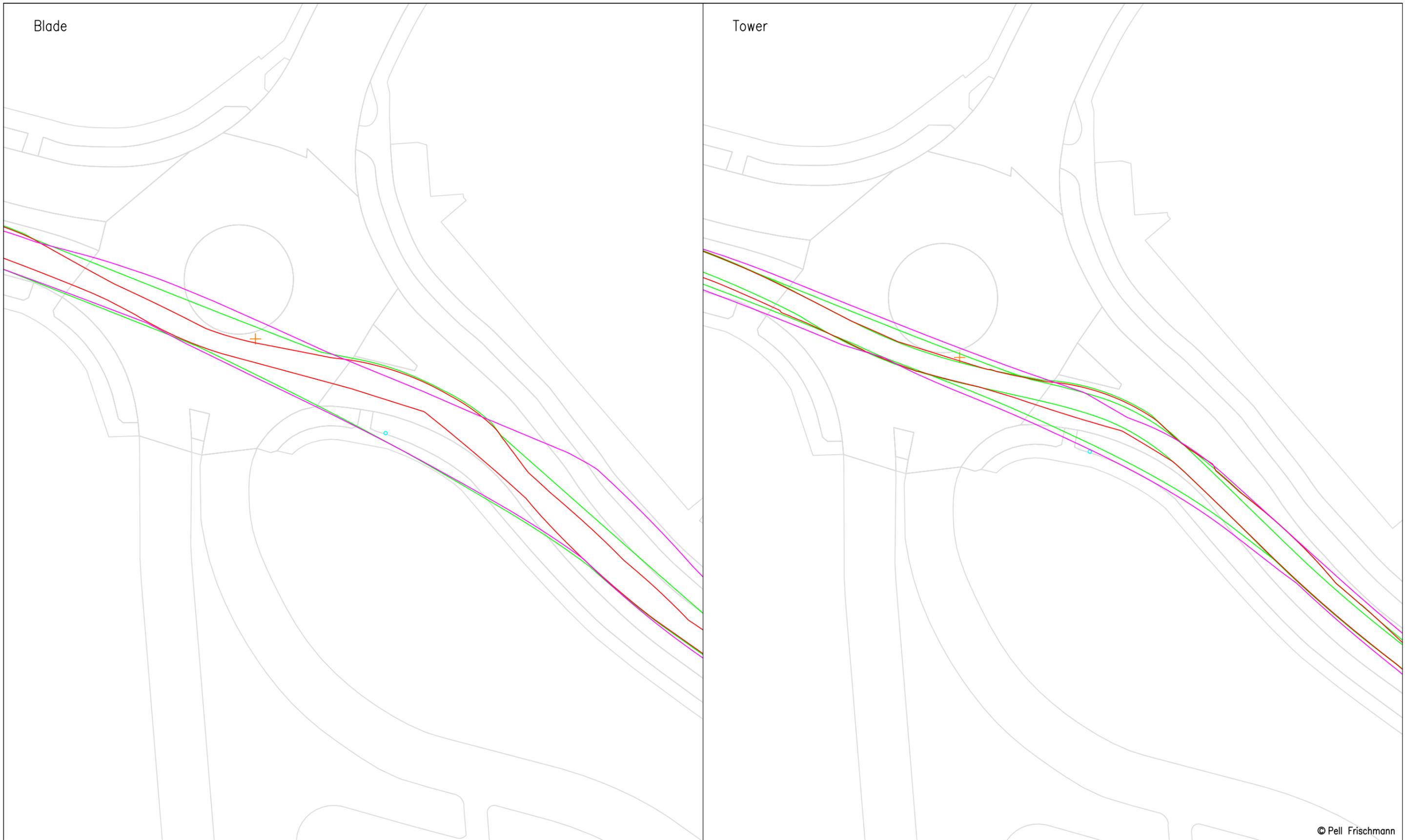


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	Kings Inch Drive Roundabout 1	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest	1		Drawing No.	SK01A	Notes:
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

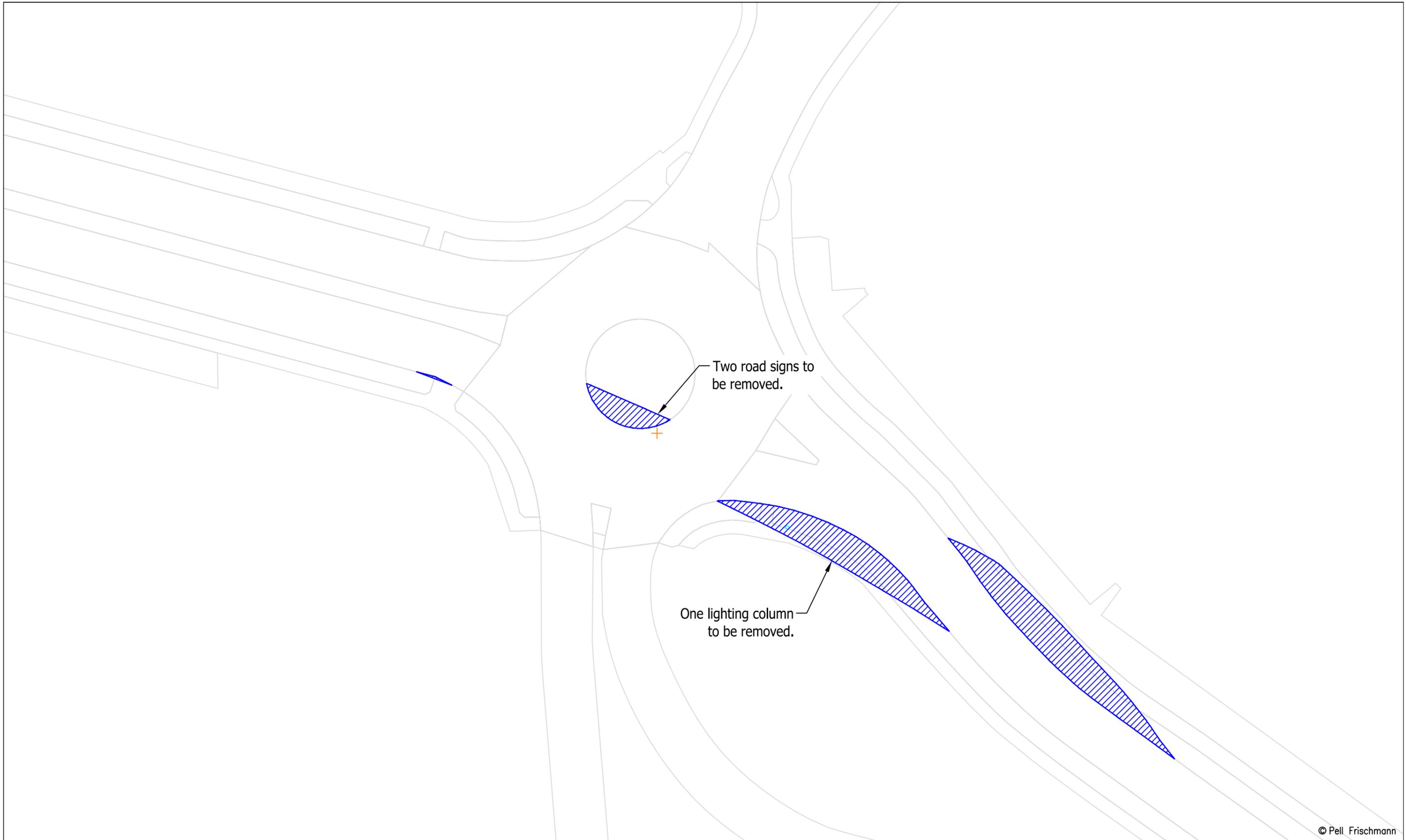
Blade

Tower



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	Client	Statkraft UK Limited	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg		
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft		
	SPA Location	Kings Inch Drive Roundabout 2	Point of Interest	2		Drawing No.	SK02	Notes:	Revision
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.		XXX

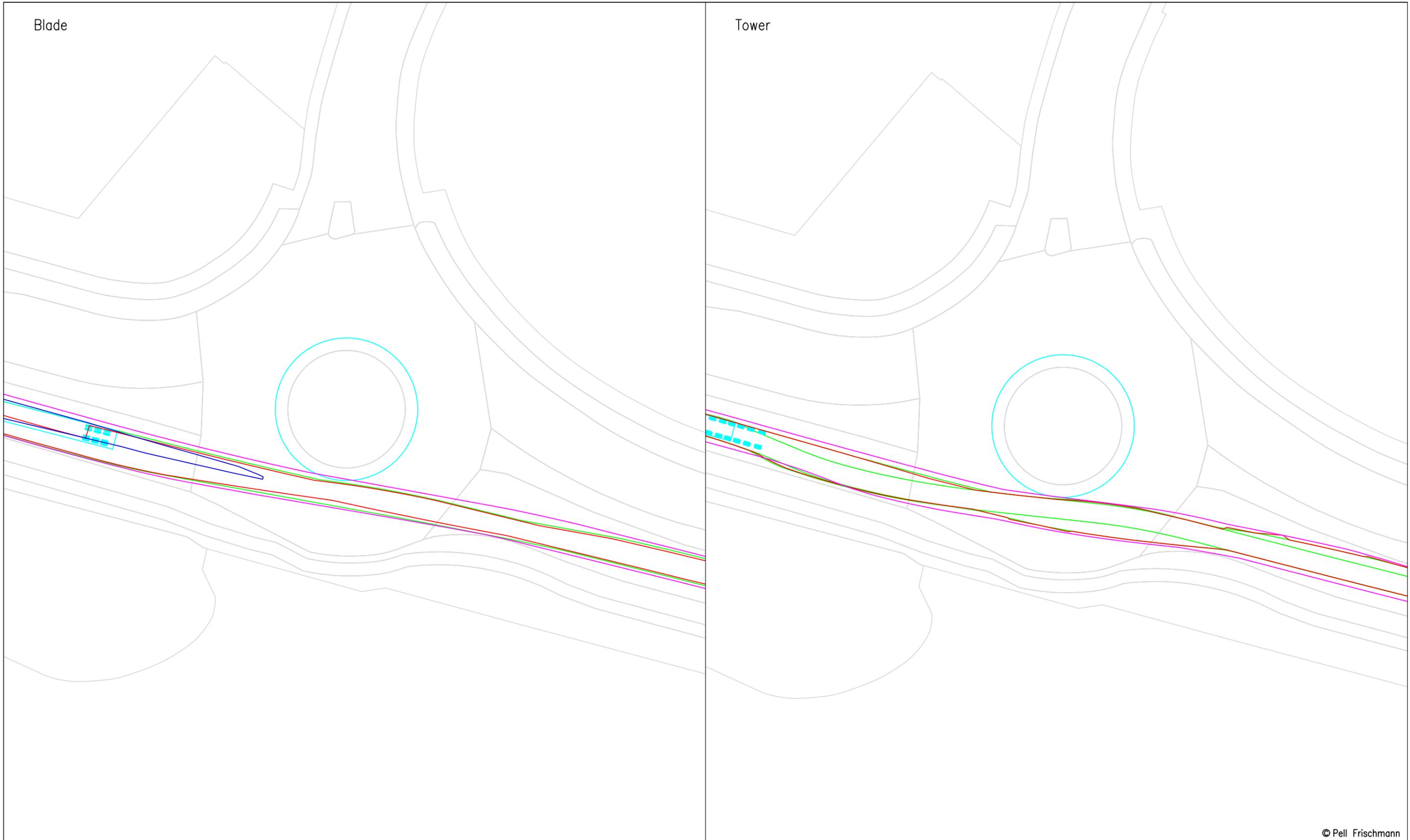


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg	
	Key	— Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	Kings Inch Drive Roundabout 2	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest			2	Drawing No.	SK02A	Revision	
			Notes:							XXX
			1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.							

Blade

Tower



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Project

Oliver Forest Wind Farm

	Name	Date	Scale
Drawn	AD	26/09/2023	1:500 @ A3
Designed	GB	26/09/2023	File No. 230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	Drawing Status
			Draft
	Point of Interest	3	

Client **Statkraft UK Limited**

Drawing Title

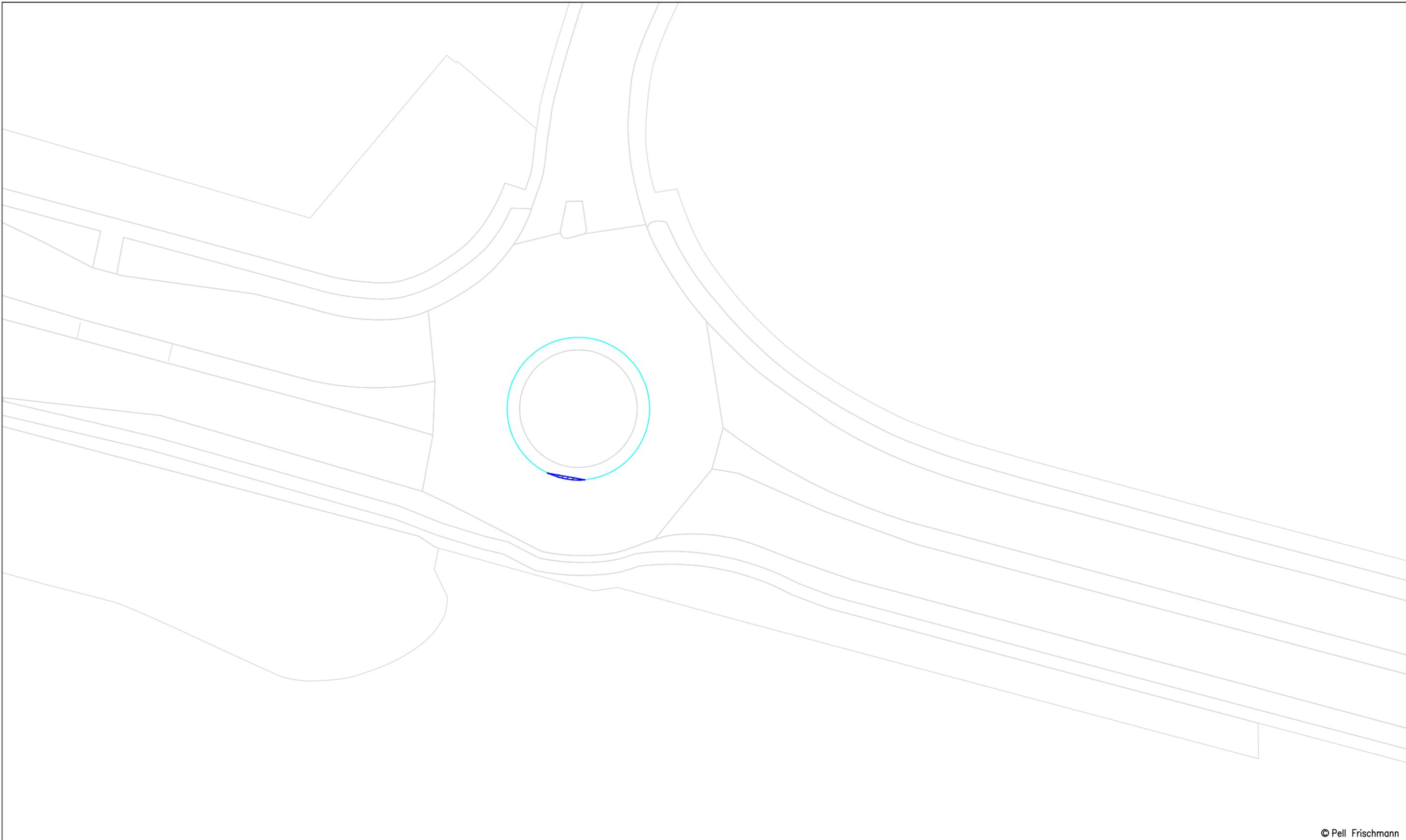
Nordex N163 Blade and Tower

Key						
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

SPA Location

Kings Inch Drive Roundabout 3

Drawing No.	Notes:	Revision
SK03	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

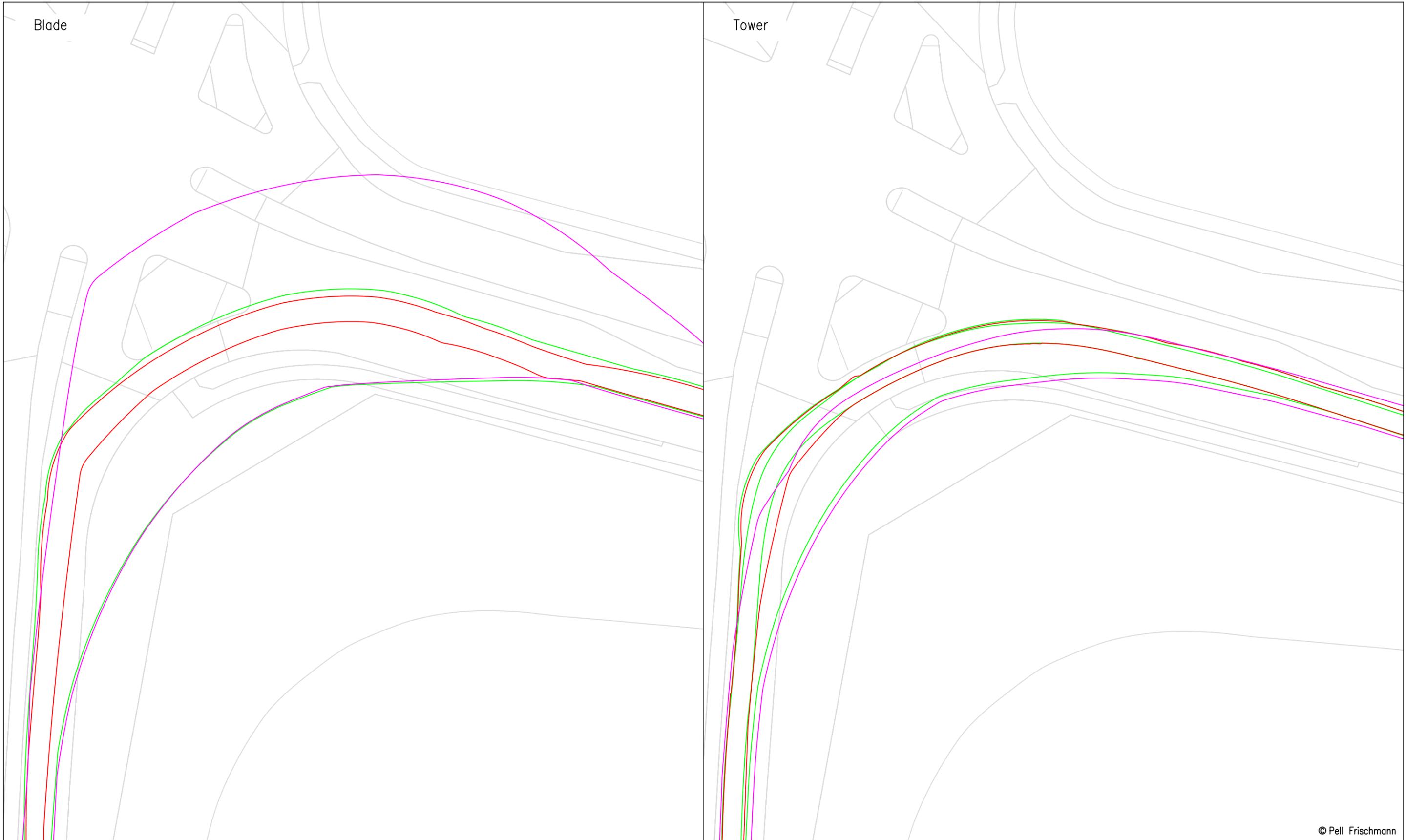


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	Kings Inch Drive Roundabout 3	Checked	GB	26/09/2023	Point of Interest	3	Drawing Status	Draft
			Drawing No.	SK03A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision

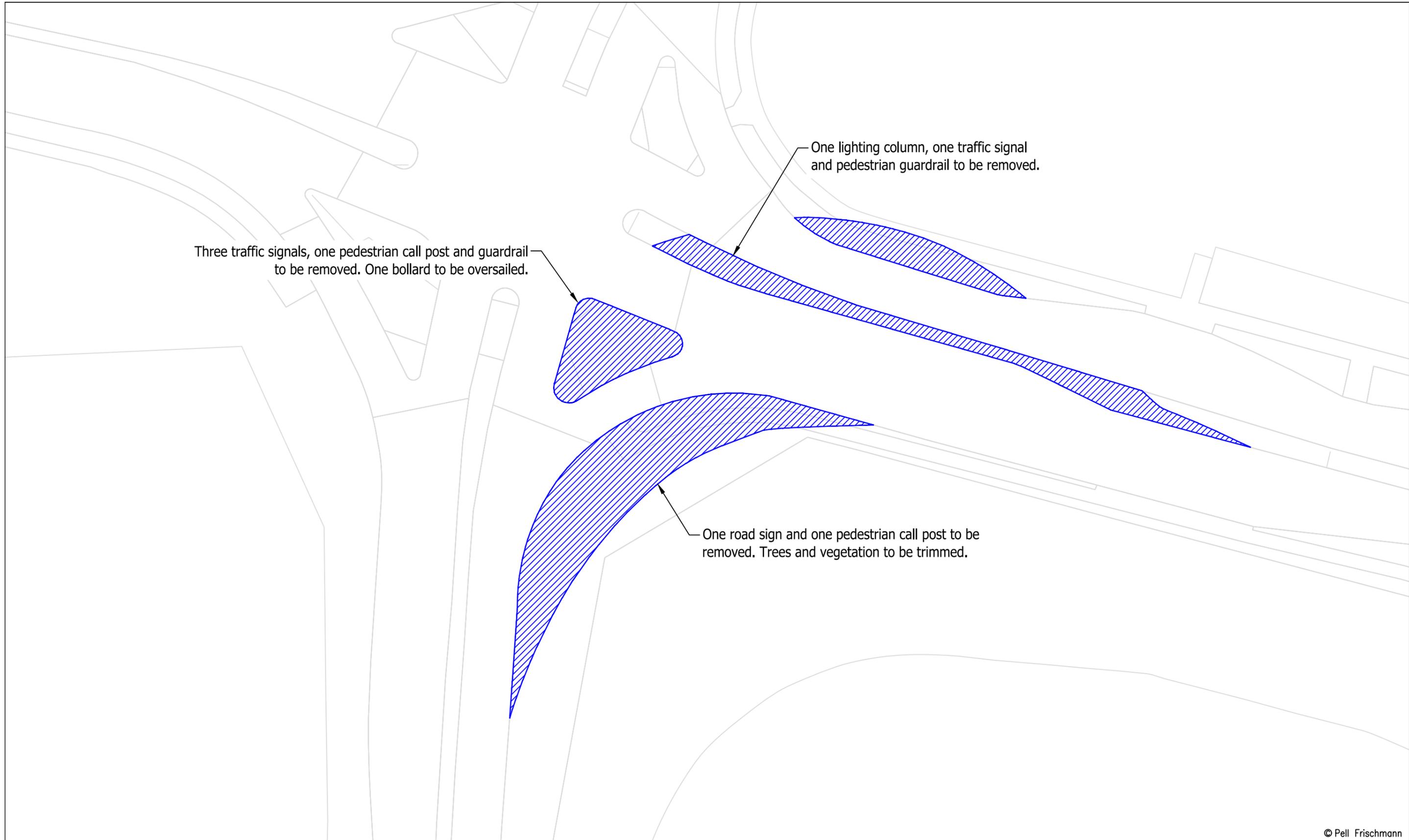
Blade

Tower



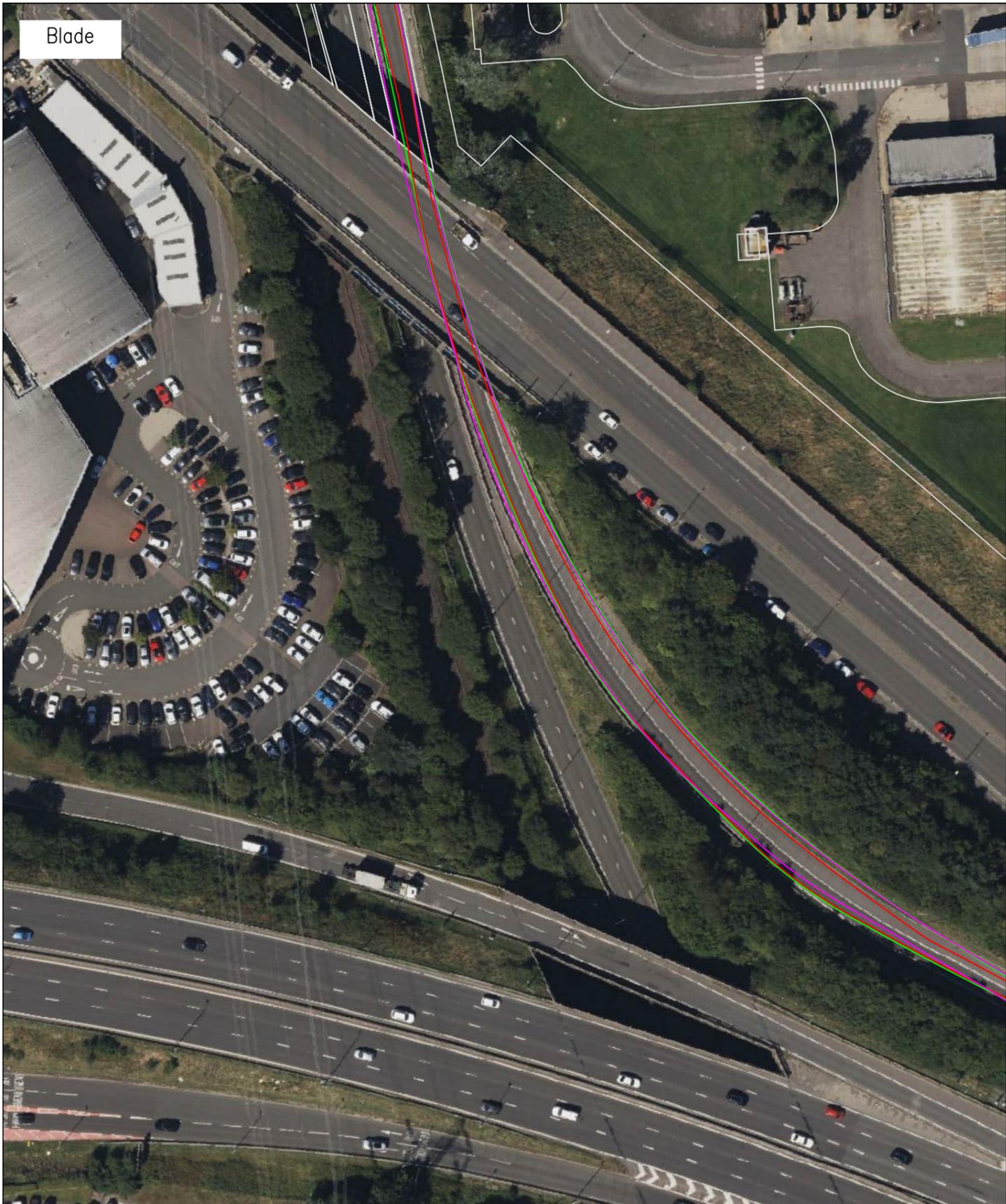
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	Kings Inch Drive / Mayo Avenue Junction	Checked	GB	26/09/2023	Point of Interest	4	Drawing Status	Draft
			Drawing No.	SK04	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision



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	Client	Statkraft UK Limited	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg		
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft		
	SPA Location	Kings Inch Drive / Mayo Avenue Junction	Point of Interest	4		Drawing No.	SK04A	Notes:	Revision
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.		XXX



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Client: Statkraft UK Limited

Key: — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail

Project	Oliver Forest Wind Farm
Drawing Title	Nordex N163 Blade and Tower
SPA Location	M8 Slip Road

Drawn	AD	26/09/2023	Scale	1:1000 @ A3
Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	Drawing Status	Draft
Point of Interest	5		Revision	XXX
Drawing No.	SK05		Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.

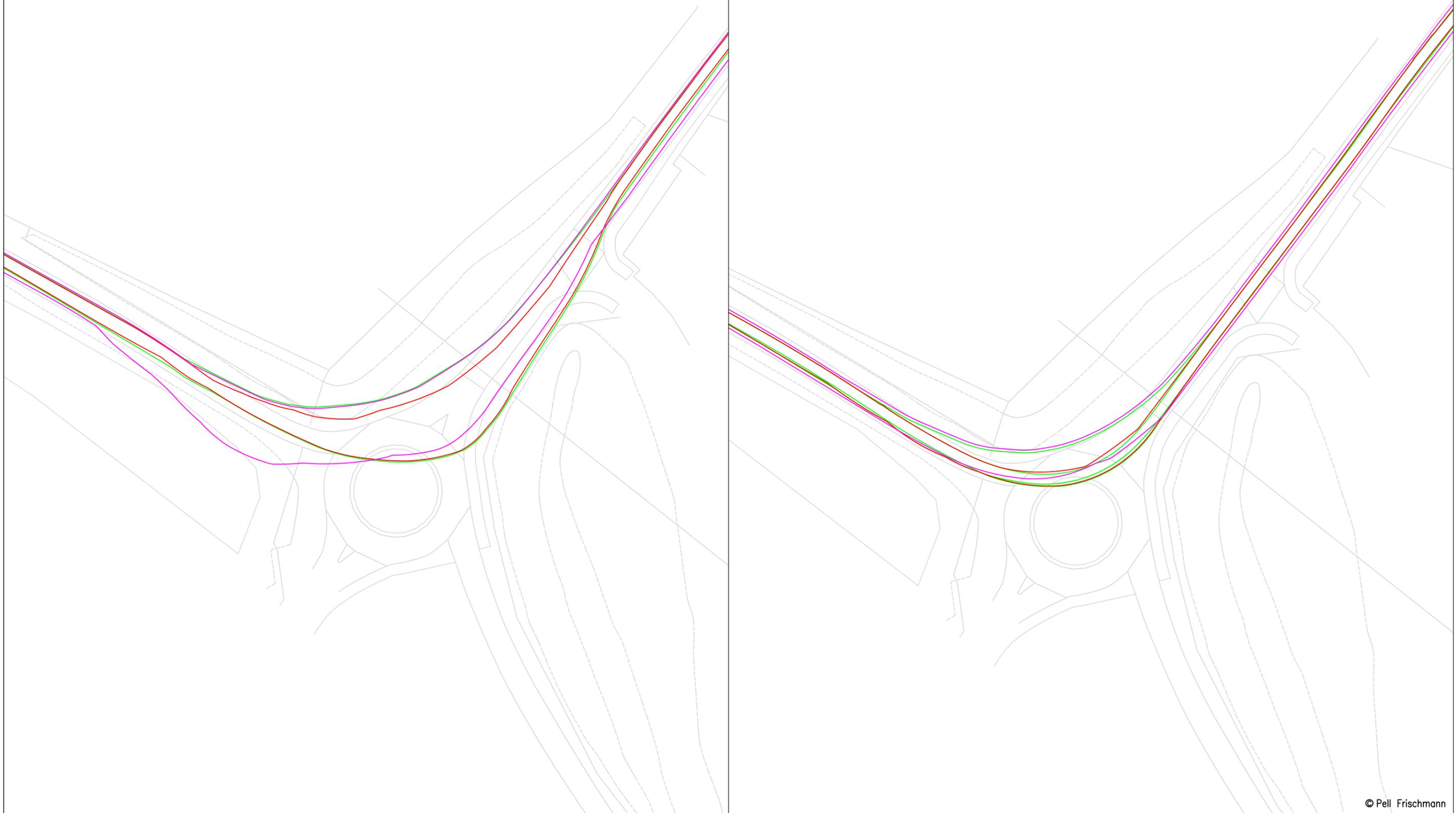


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	M8 Slip Road	Checked	GB	26/09/2023	Drawing Status	Draft	Revision	XXX
			Point of Interest	5	Drawing No.	SK05A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	

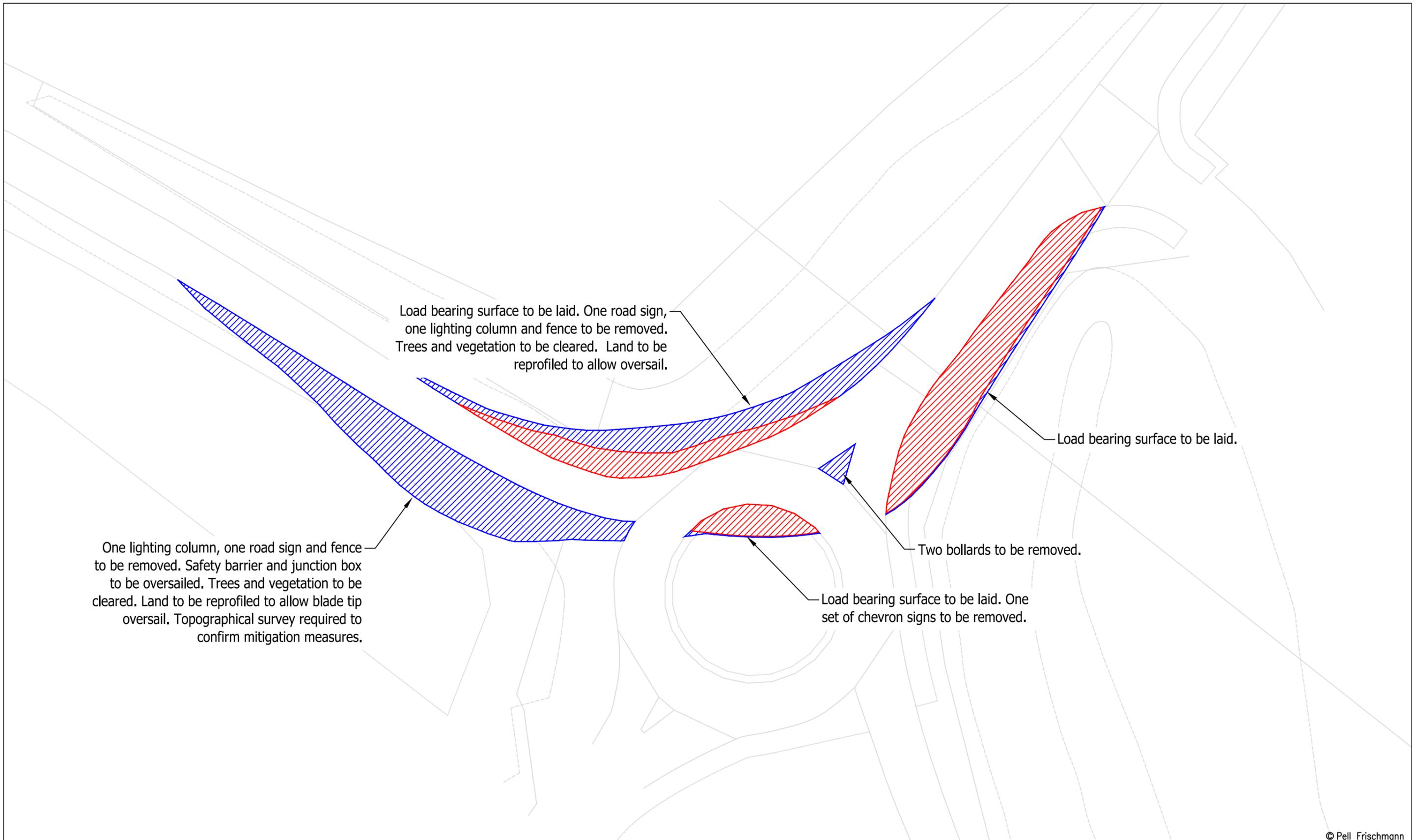
Blade

Tower



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	Client	Statkraft UK Limited	Drawn	GB	Designed	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg	
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A74(M) Junction 15 / A701 Roundabout	Checked	GB	26/09/2023	Point of Interest	6	Drawing Status	Draft
				Drawing No.	SK06	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.		Revision



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A74(M) Junction 15 / A701 Roundabout	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest	6		Drawing No.	SK06A	Notes:
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

Blade

Tower



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Project

Oliver Forest Wind Farm

	Name	Date	Scale
Drawn	AD	26/09/2023	1:1000 @ A3
Designed	GB	26/09/2023	File No 230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	
Point of Interest			Drawing Status
7			Draft

Client Statkraft UK Limited

Drawing Title

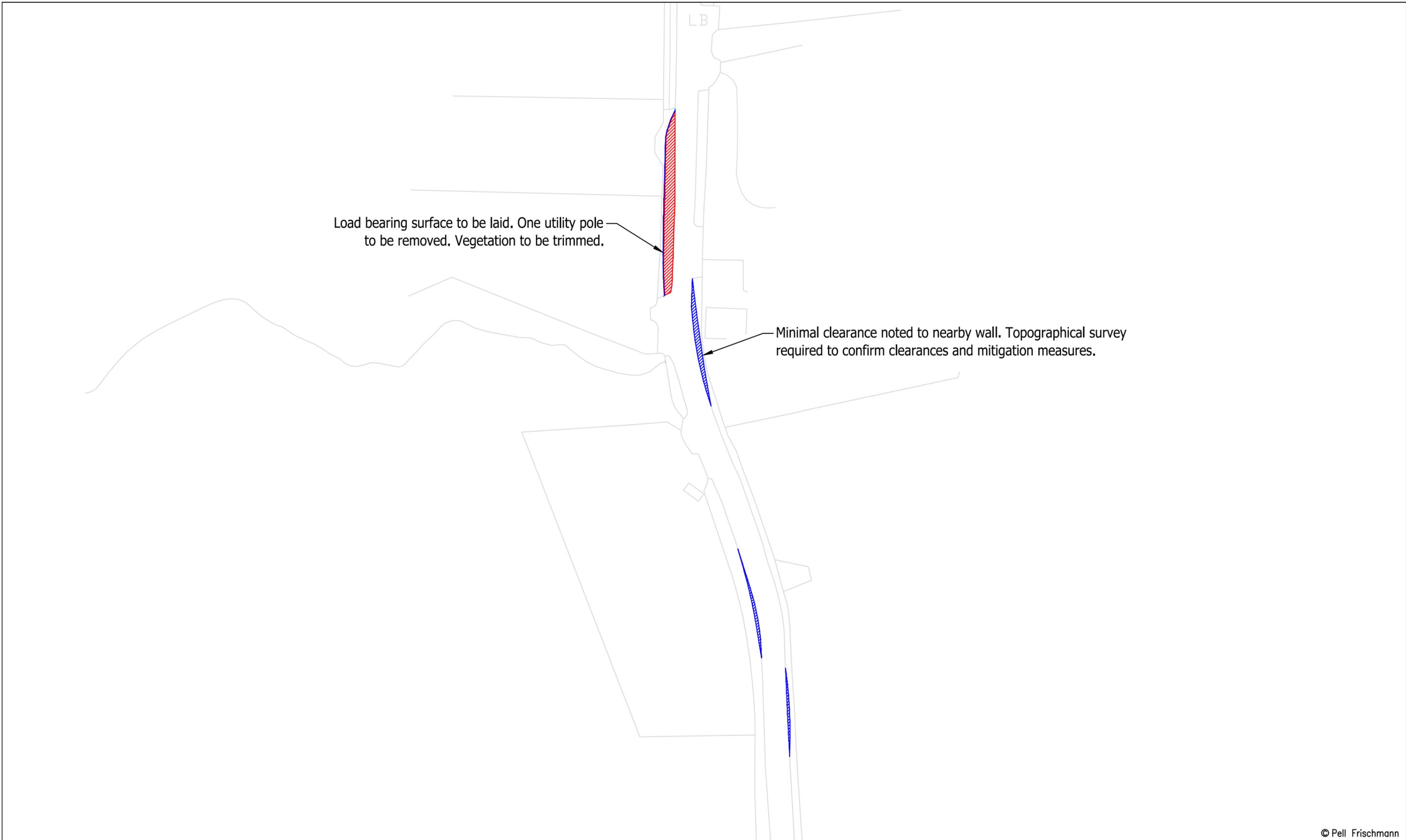
Nordex N163 Blade and Tower

Key						
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

SPA Location

A701 Entering Moffat

Drawing No.	Notes:	Revision
SK07	<ol style="list-style-type: none"> All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 	XXX



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 Entering Moffat	Checked	GB	26/09/2023	Point of Interest	7	Drawing Status	Draft
			Drawing No.	SK07A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision

Blade

Annan Bridge



Tower

Annan Bridge



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Project

Oliver Forest Wind Farm

Drawn	AD	26/09/2023	Scale	1:1000 @ A3
Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	Drawing Status	Draft
Point of Interest	8		Revision	XXX

Client Statkraft UK Limited

Drawing Title

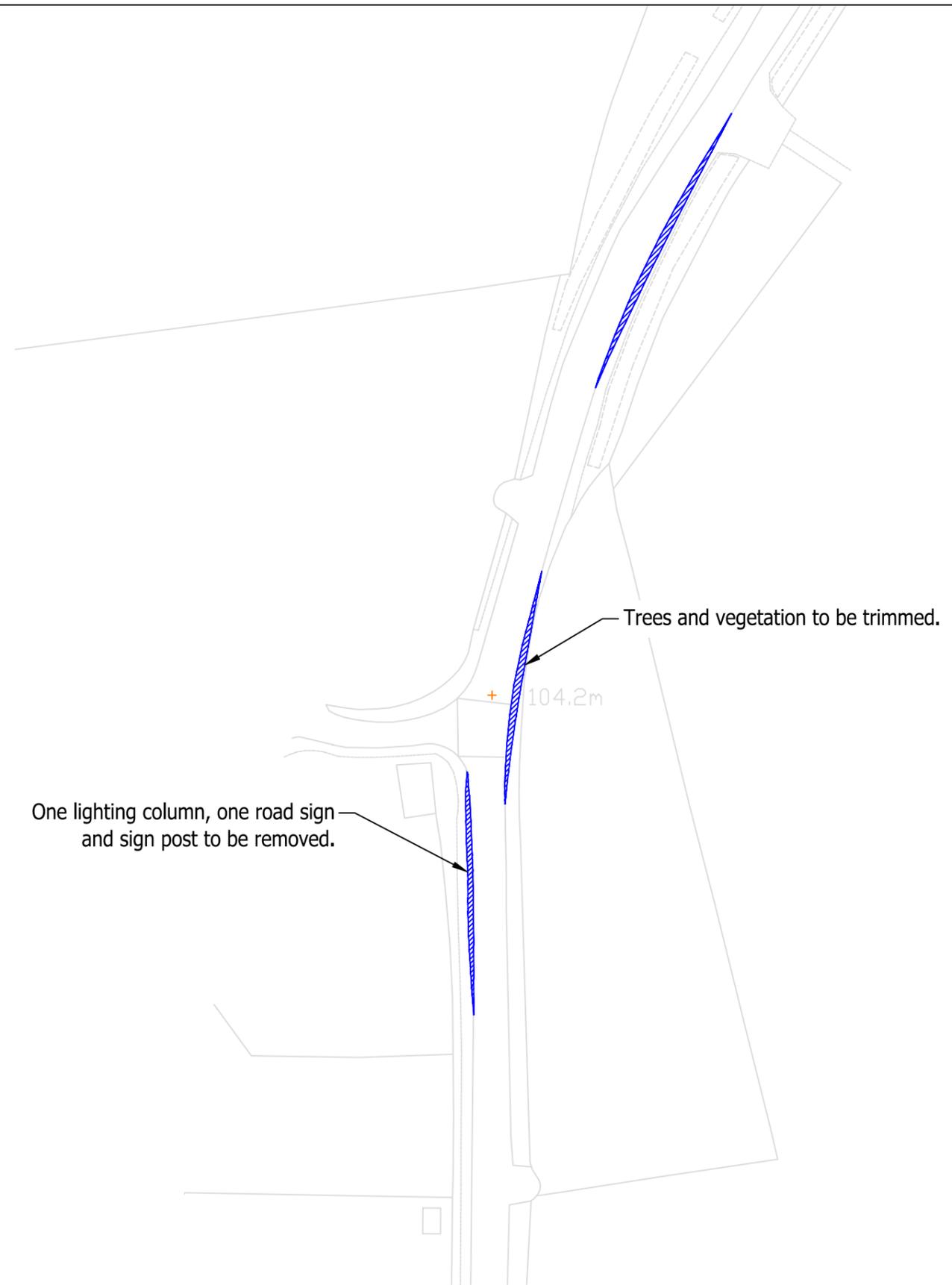
Nordex N163 Blade and Tower

Key	—	—	—	—		
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

SPA Location

A701 Entering Moffat

Drawing No.	SK08	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 Entering Moffat	Checked	GB	26/09/2023	Point of Interest	8	Drawing Status	Draft
			Drawing No.	SK08A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision

Blade

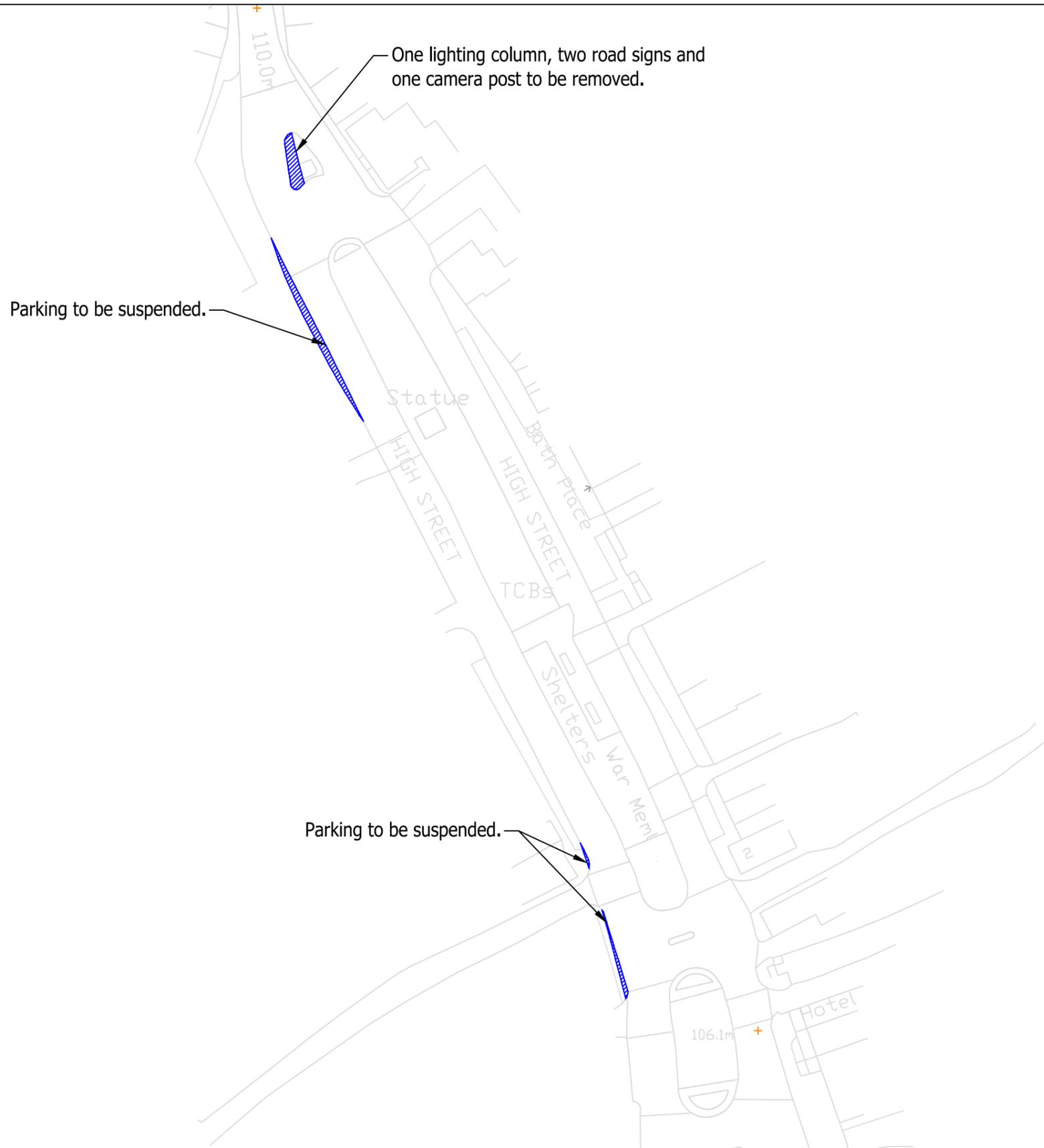


Tower



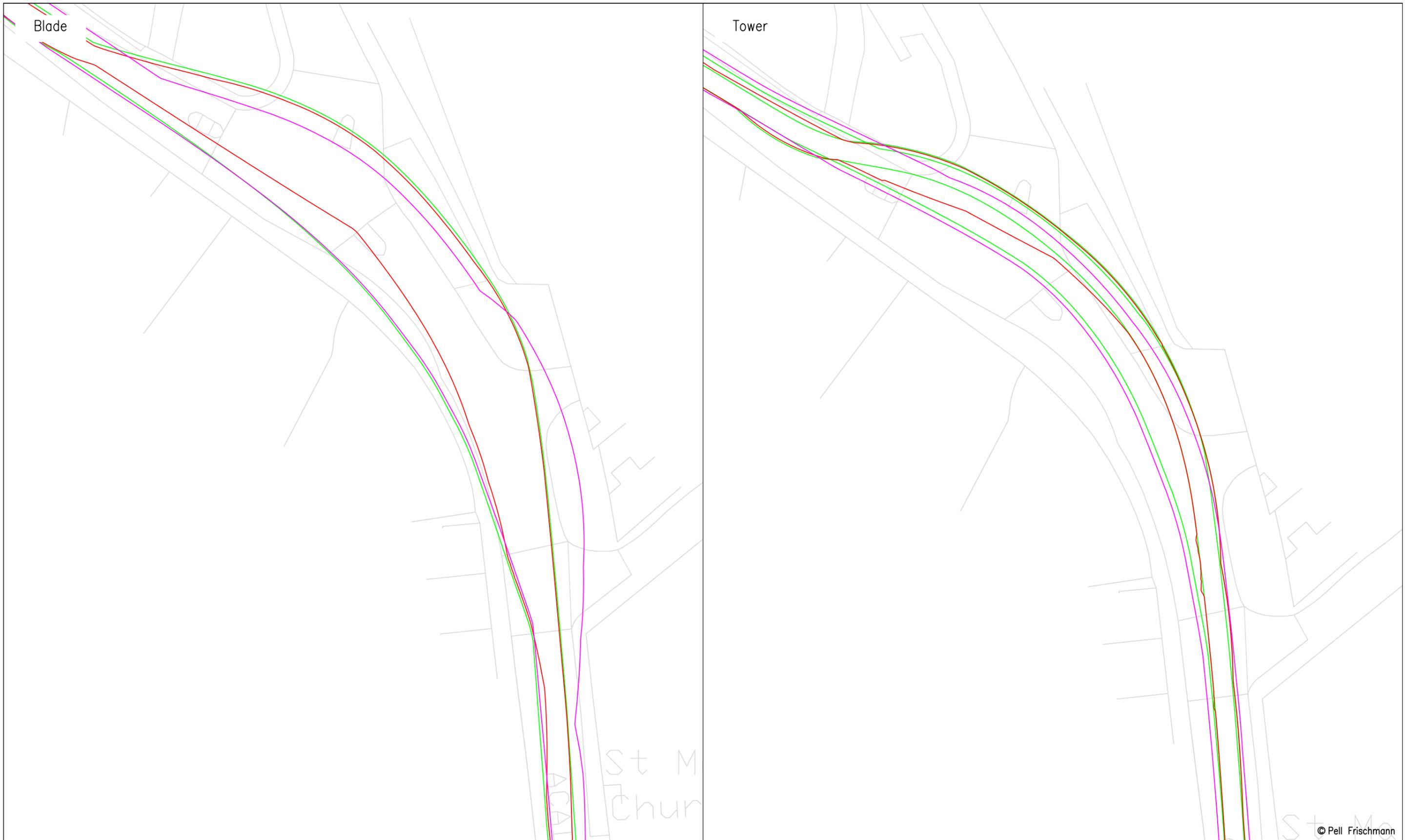
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 Moffat	Checked	GB	26/09/2023	Drawing Status	Draft		
				Point of Interest	9		Drawing No.	SK09	Revision
			Notes:		1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.				



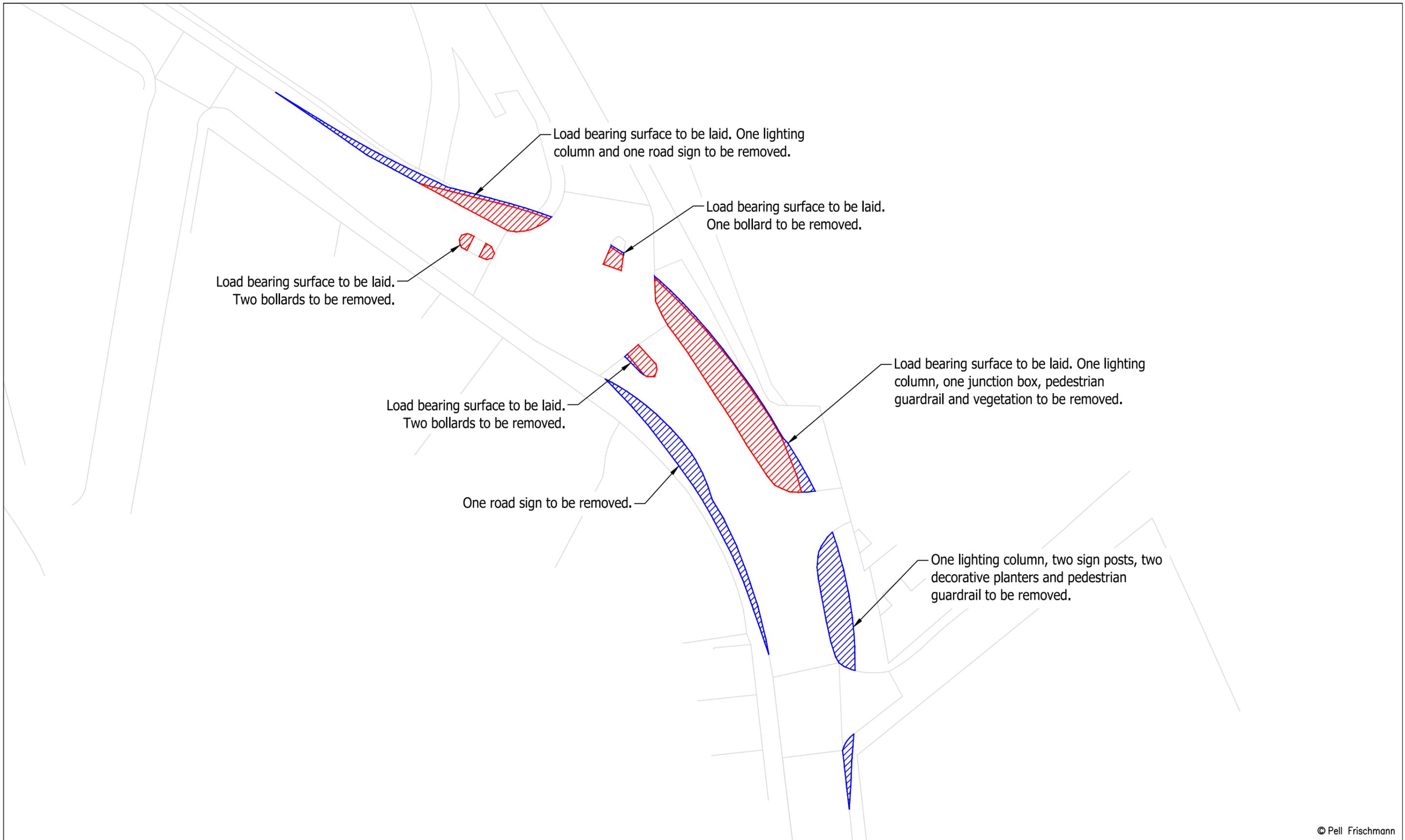
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No	230912 Oliver Forest SPA N163 FINAL.dwg
Key 	SPA Location	A701 Moffat	Checked	GB	26/09/2023	Point of Interest	9	Drawing Status	Draft
			Drawing No.	SK09A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision



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	Client	Statkraft UK Limited	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg		
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft		
	SPA Location	A701 High Street and Academy Road Junction	Point of Interest	10		Drawing No.	SK10	Revision	XXX
			Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.					

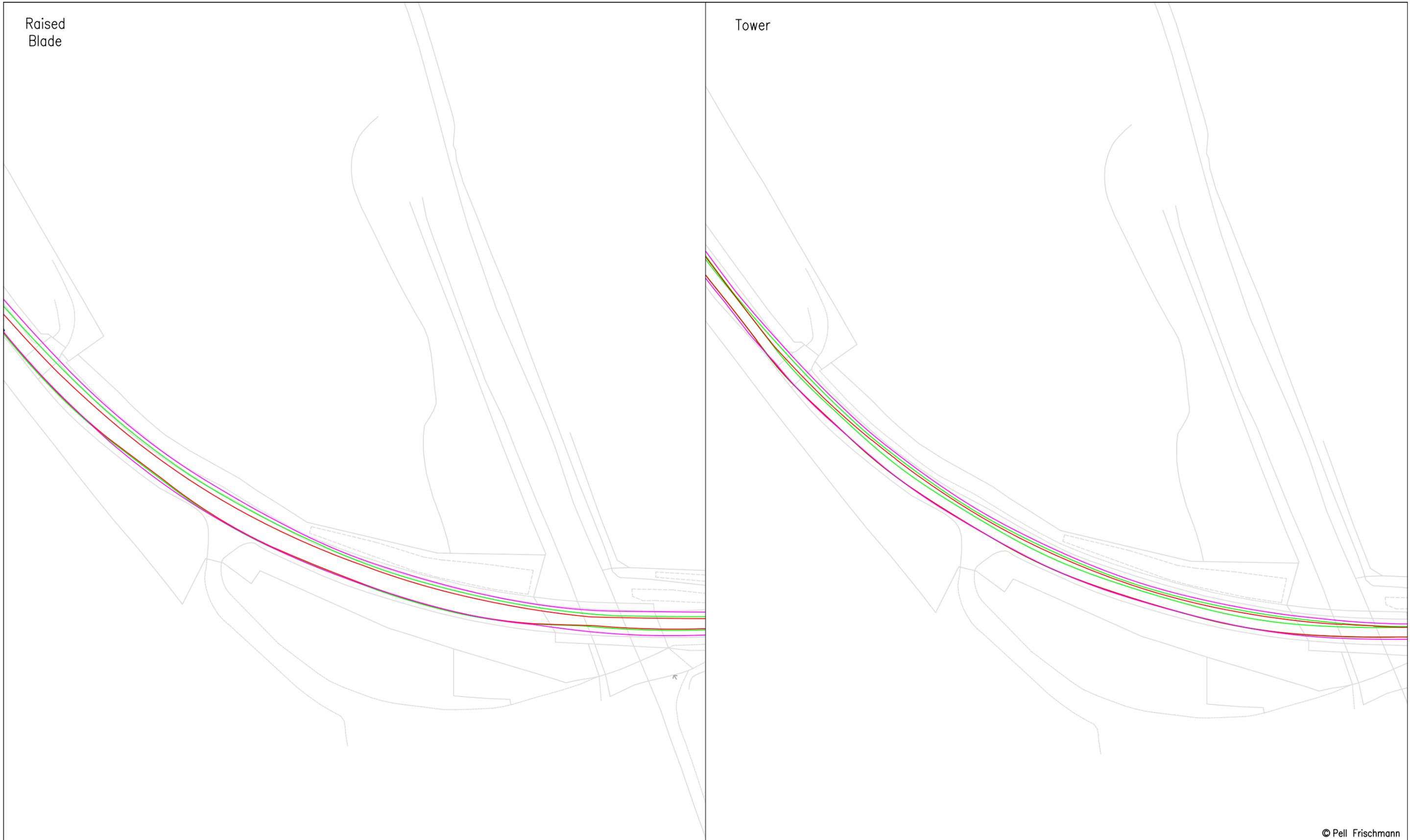


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	Client	Statkraft UK Limited	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg	
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft	
	SPA Location	A701 High Street and Academy Road Junction	Point of Interest	10		Drawing No.	SK10A	
			Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.				Revision

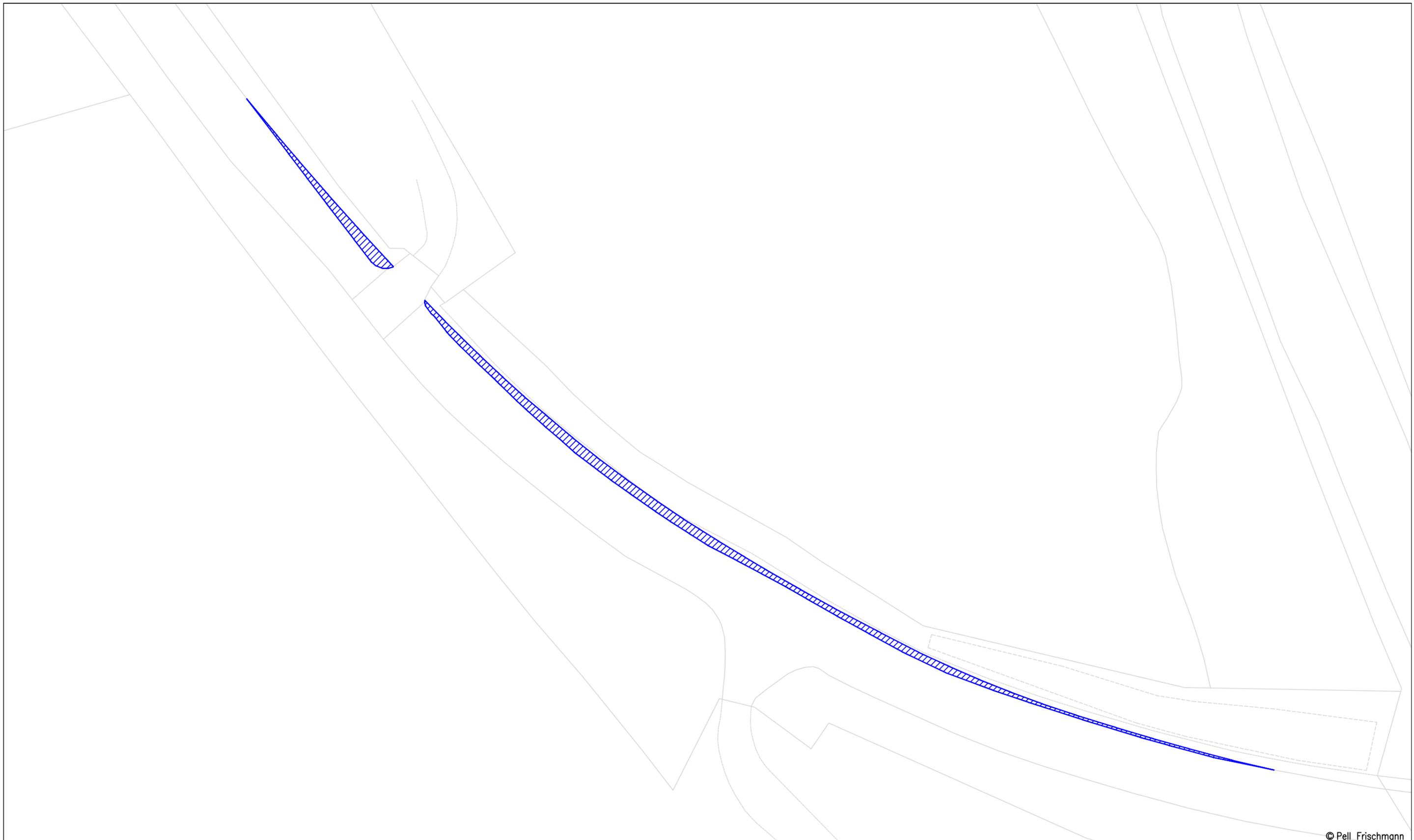
Raised
Blade

Tower



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	Client	Statkraft UK Limited	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg		
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft		
	SPA Location	A701 South of Moffat Cemetery	Point of Interest	11		Drawing No.	SK11	Notes:	Revision
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.		XXX

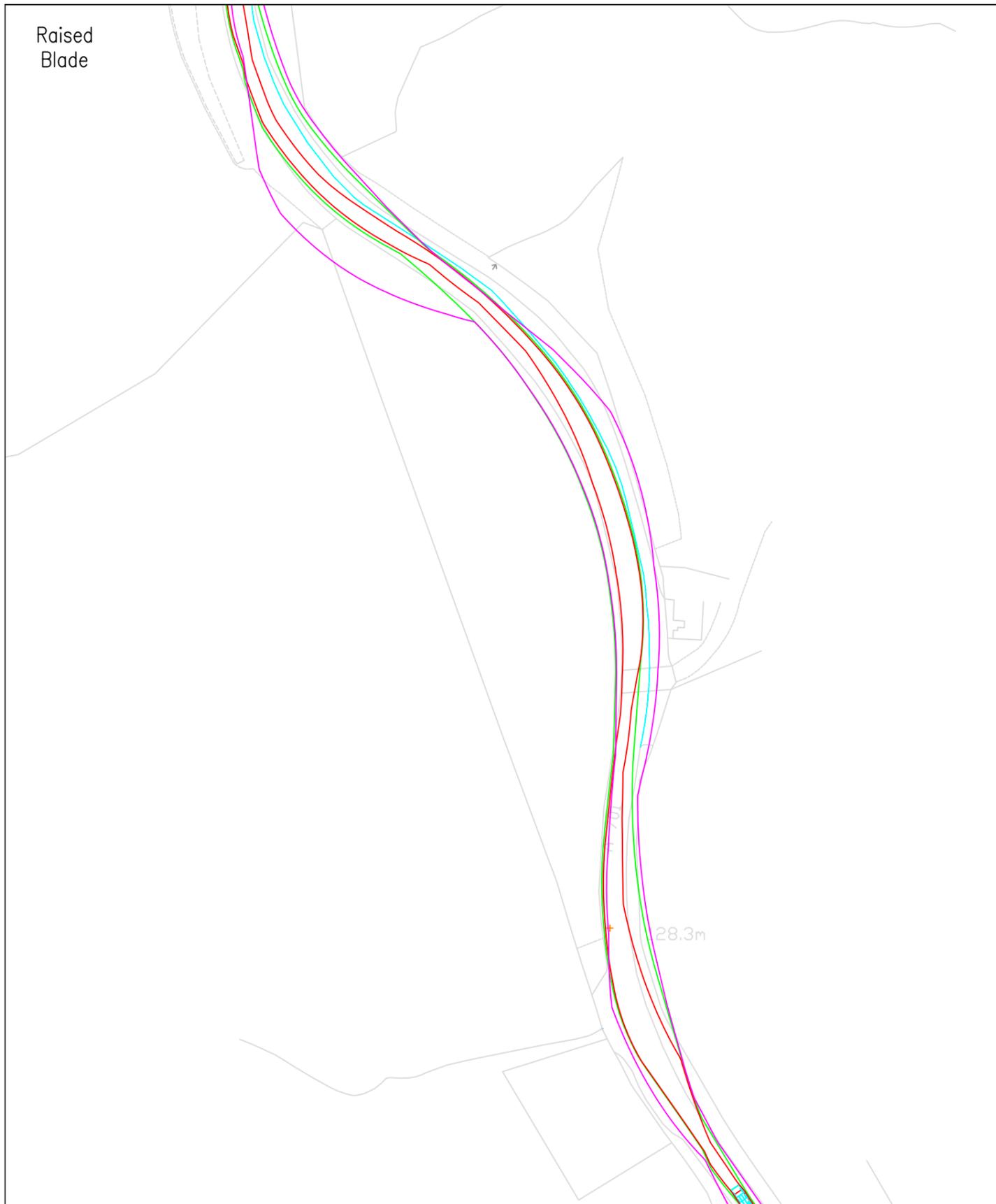


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key  Wheel SPA  Body SPA  Load SPA  Indicative  Over-run  Over-sail	SPA Location	A701 South of Moffat Cemetery	Checked	GB	26/09/2023	Drawing Status	Draft	Revision	XXX
				Point of Interest		11	Drawing No.	SK11A	Notes:

Raised
Blade

Tower



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Client **Statkraft UK Limited**

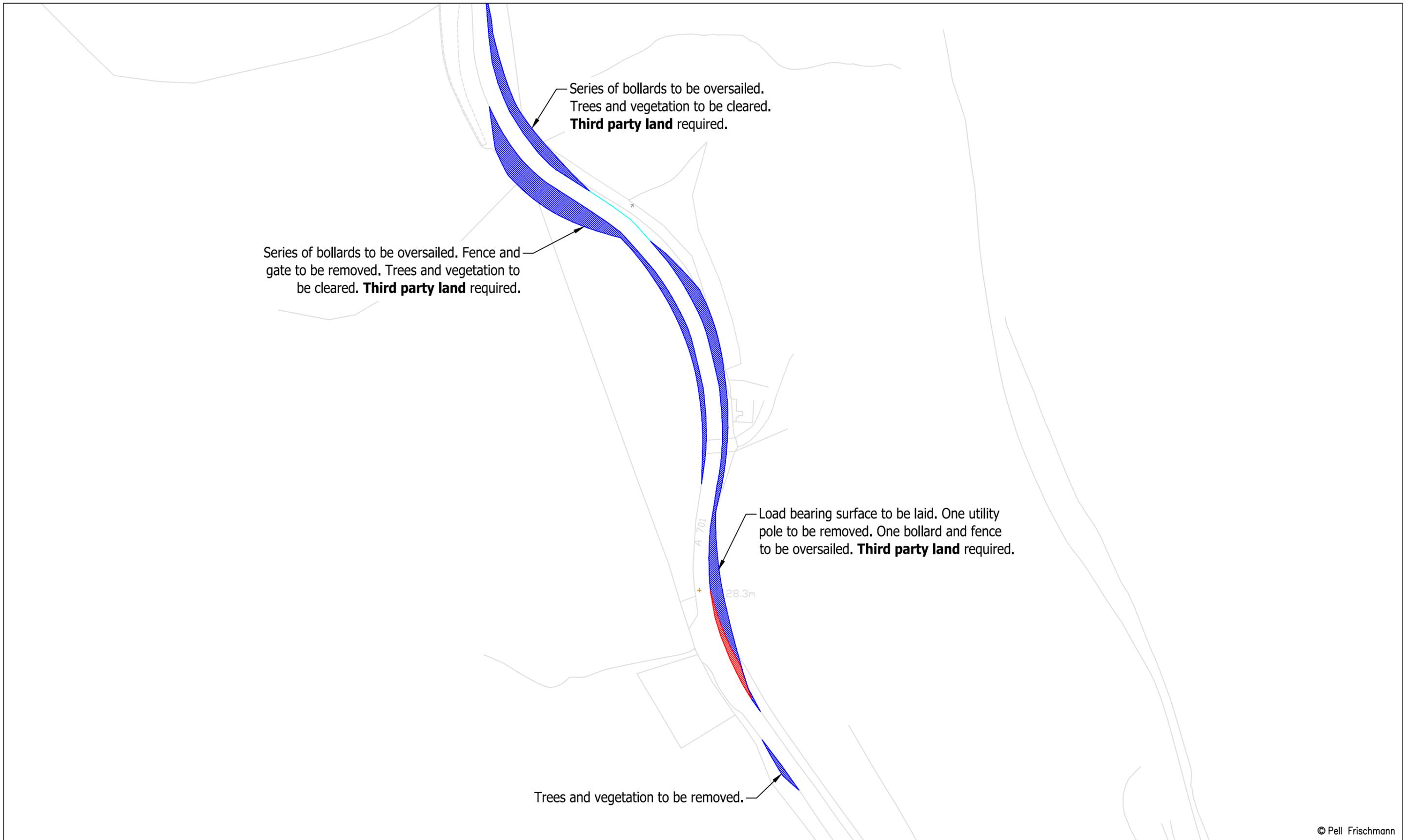
Key	—	—	—	—		
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

Project
Oliver Forest Wind Farm

Drawing Title
Nordex N163 Blade and Tower

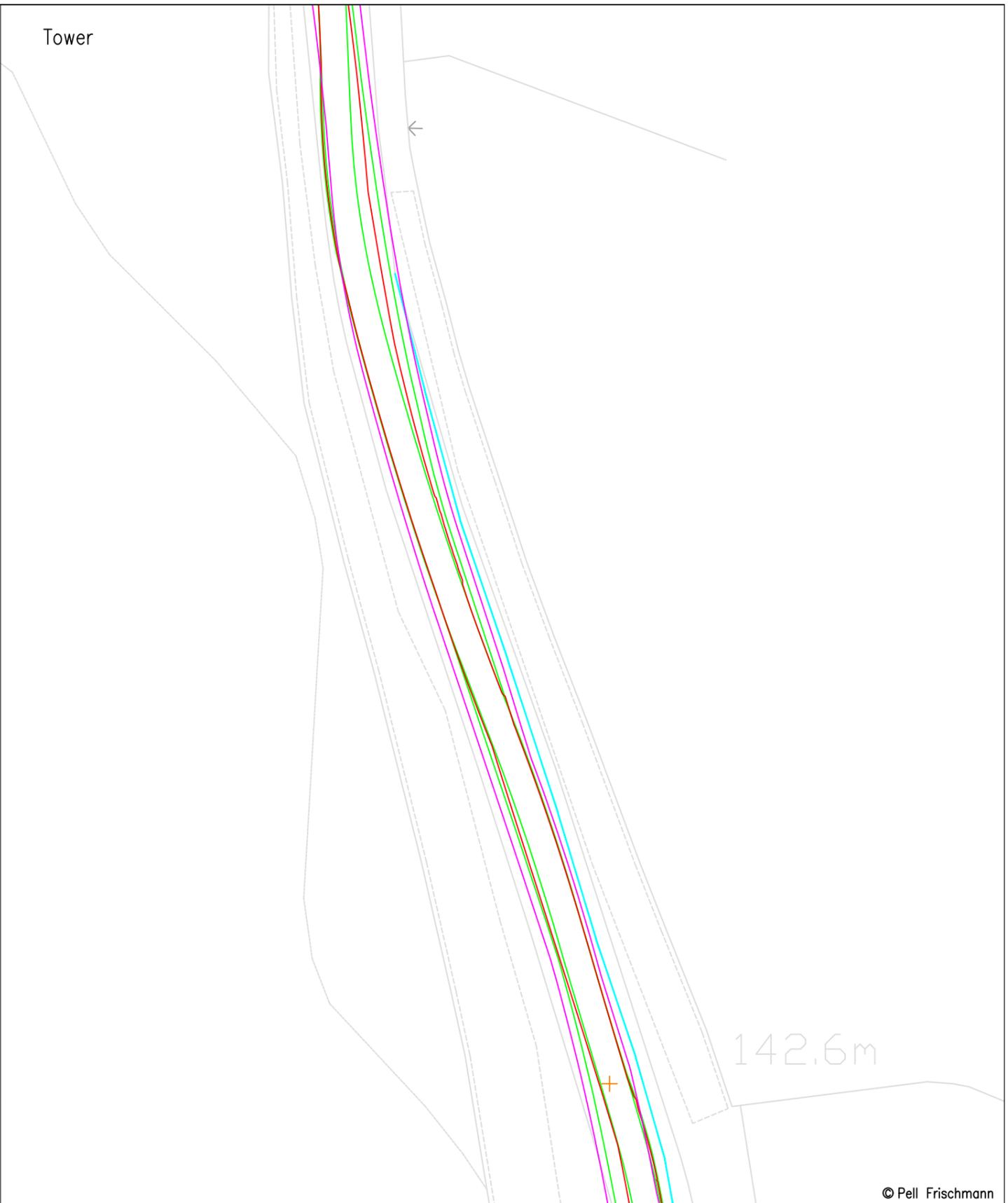
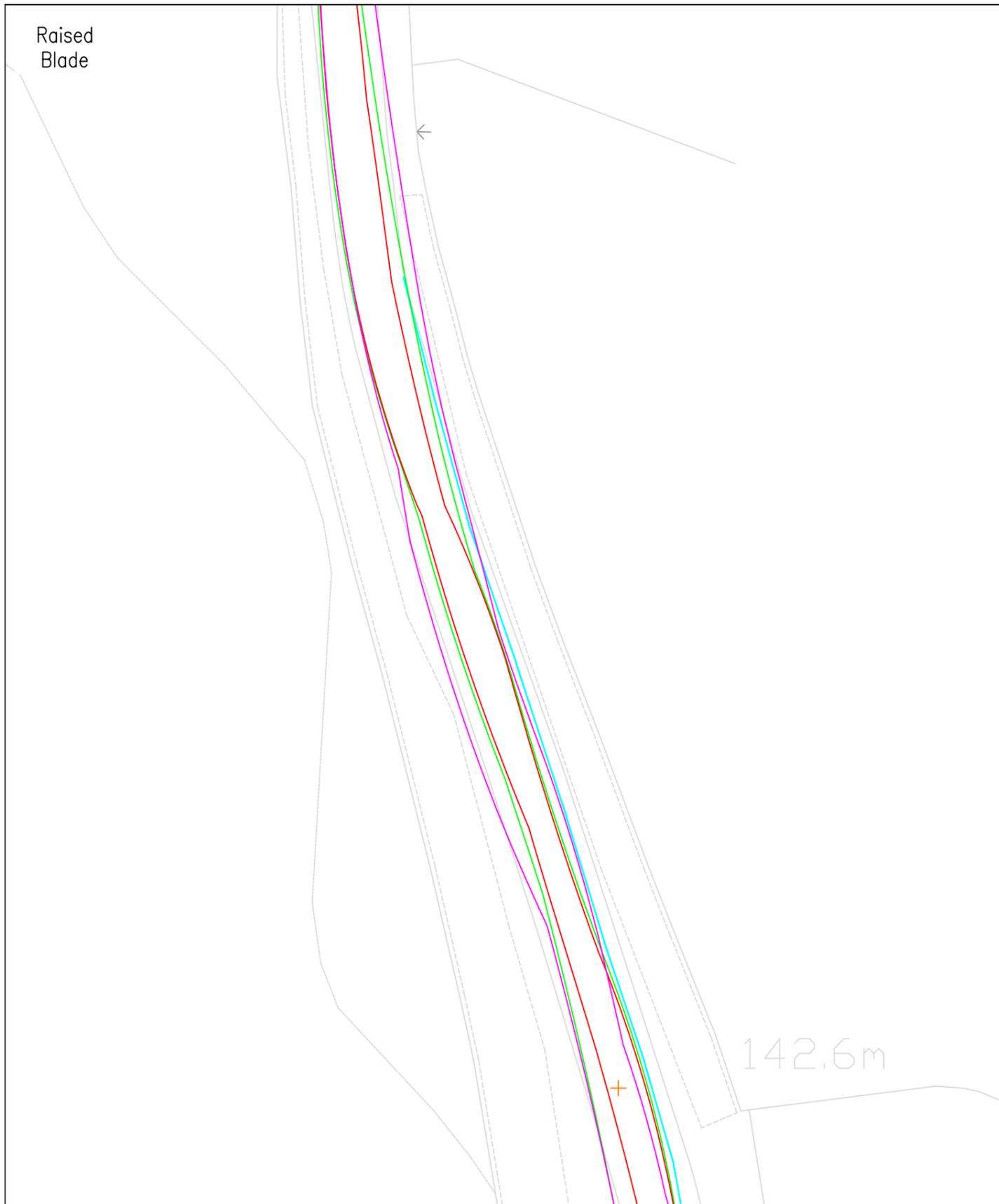
SPA Location
A701 Moffat Cemetery

	Name	Date	Scale	1:1250 @ A3
Drawn	AD	26/09/2023	File No 230912 Oliver Forest SPA N163 FINAL.dwg	Drawing Status Draft
Designed	GB	26/09/2023		
Checked	GB	26/09/2023		
Point of Interest		12	Revision	
Drawing No.	Notes:			Revision
SK12	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			XXX



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	Client	Statkraft UK Limited	Designated	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg	
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft	
	SPA Location	A701 Moffat Cemetery	Point of Interest	12		Drawing No.	SK12A	Notes:
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 North of Moffat Cemetery	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest	13		Drawing No.	SK13	Notes:
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

Blade

159.4m

Tower

159.4m

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Project

Oliver Forest Wind Farm

Drawn	AD	26/09/2023	Scale	1:1000 @ A3	
Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg	
Checked	GB	26/09/2023	Drawing Status	Draft	
Point of Interest		14			
Drawing No.	SK14			Notes:	Revision
				1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

Client Statkraft UK Limited

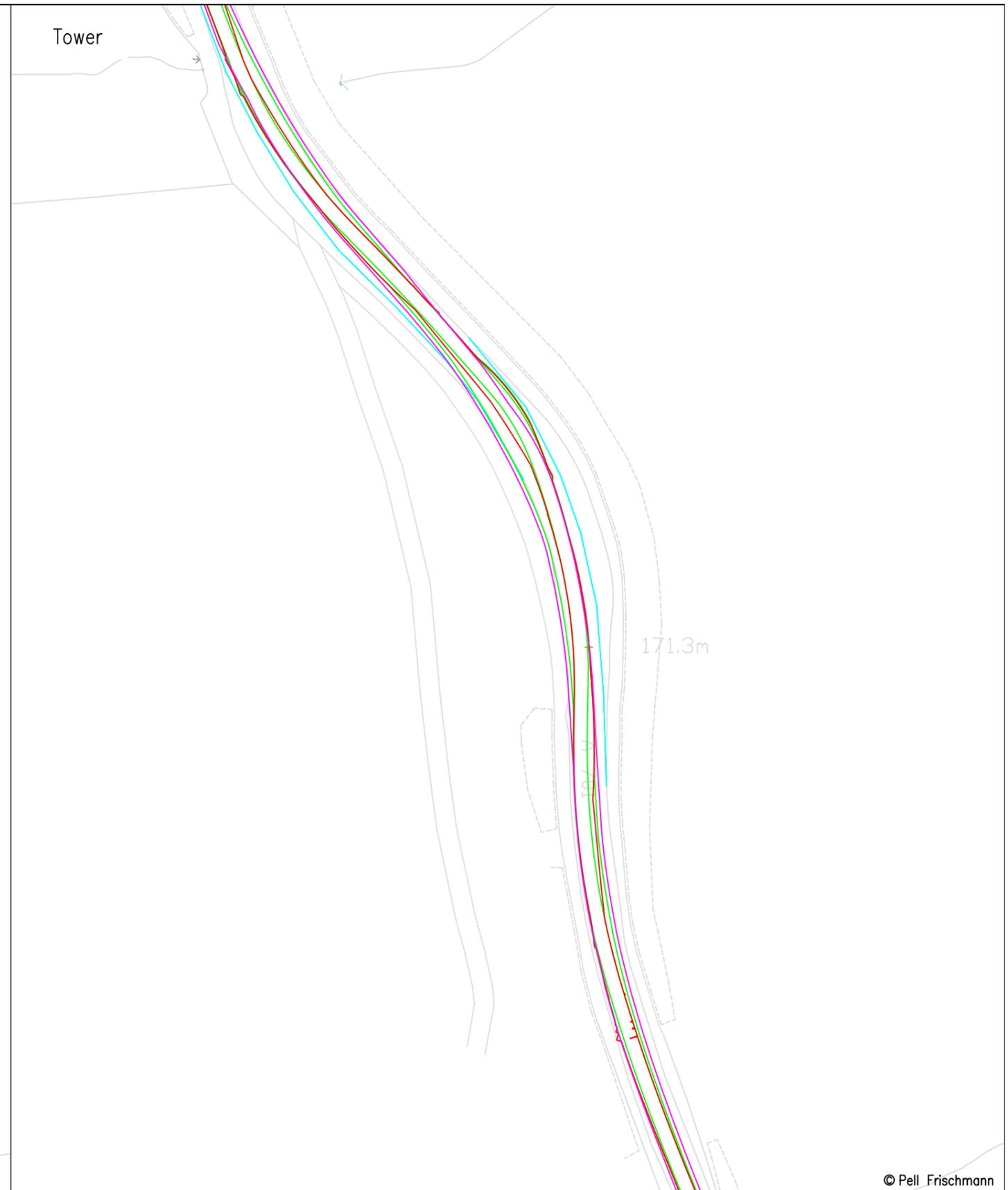
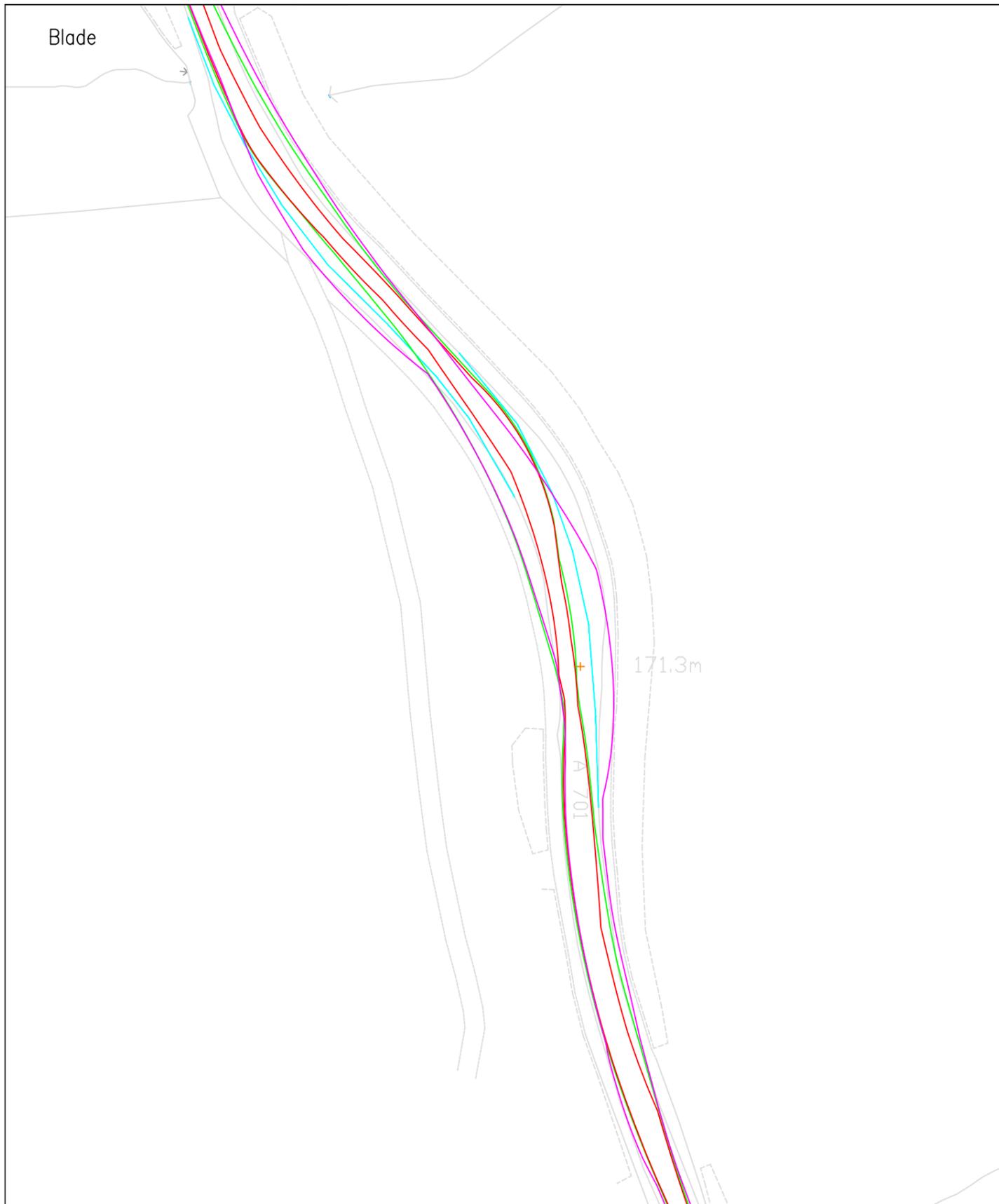
Drawing Title

Nordex N163 Blade and Tower

Key						
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

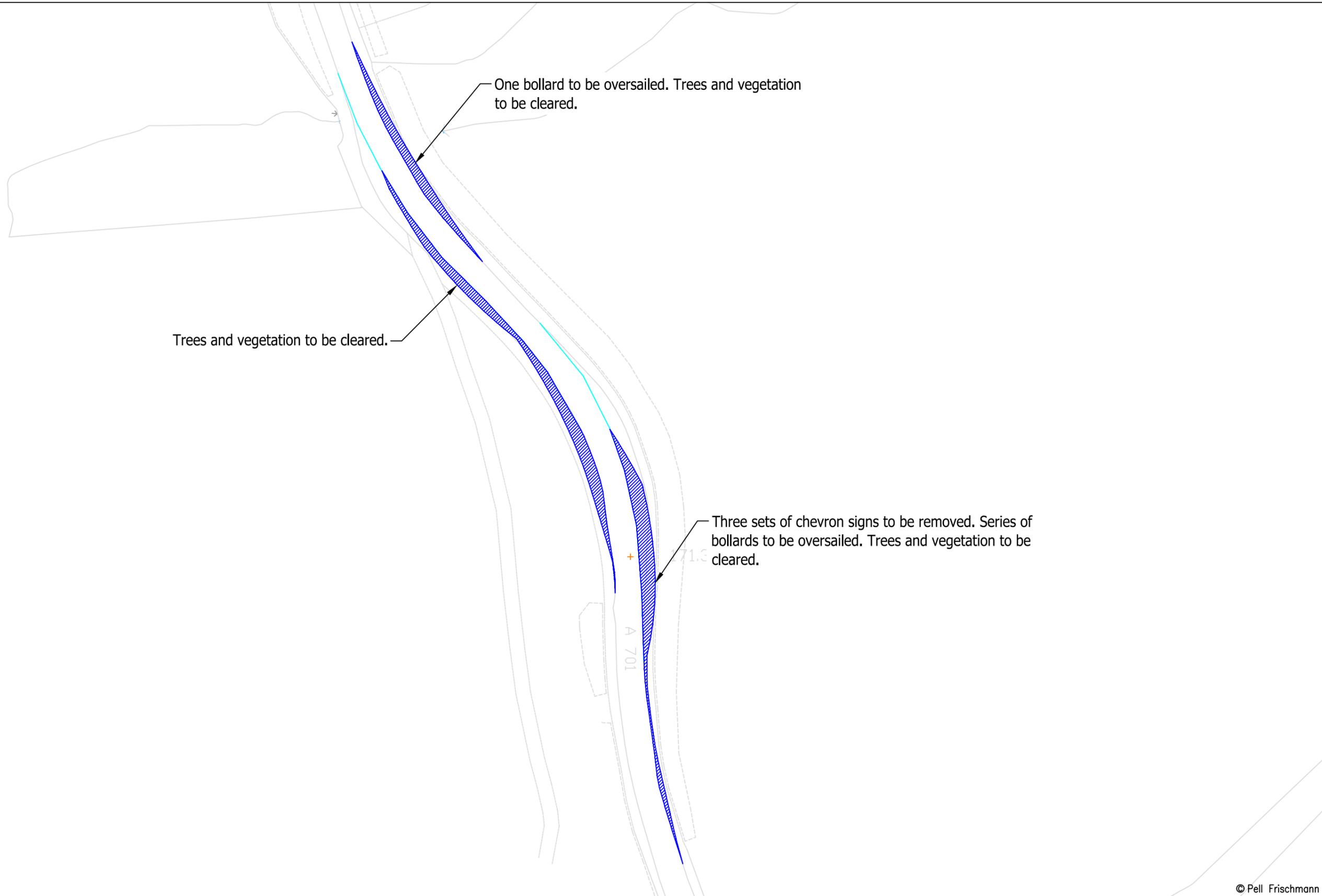
SPA Location

A701 Bends South of Adamsholm



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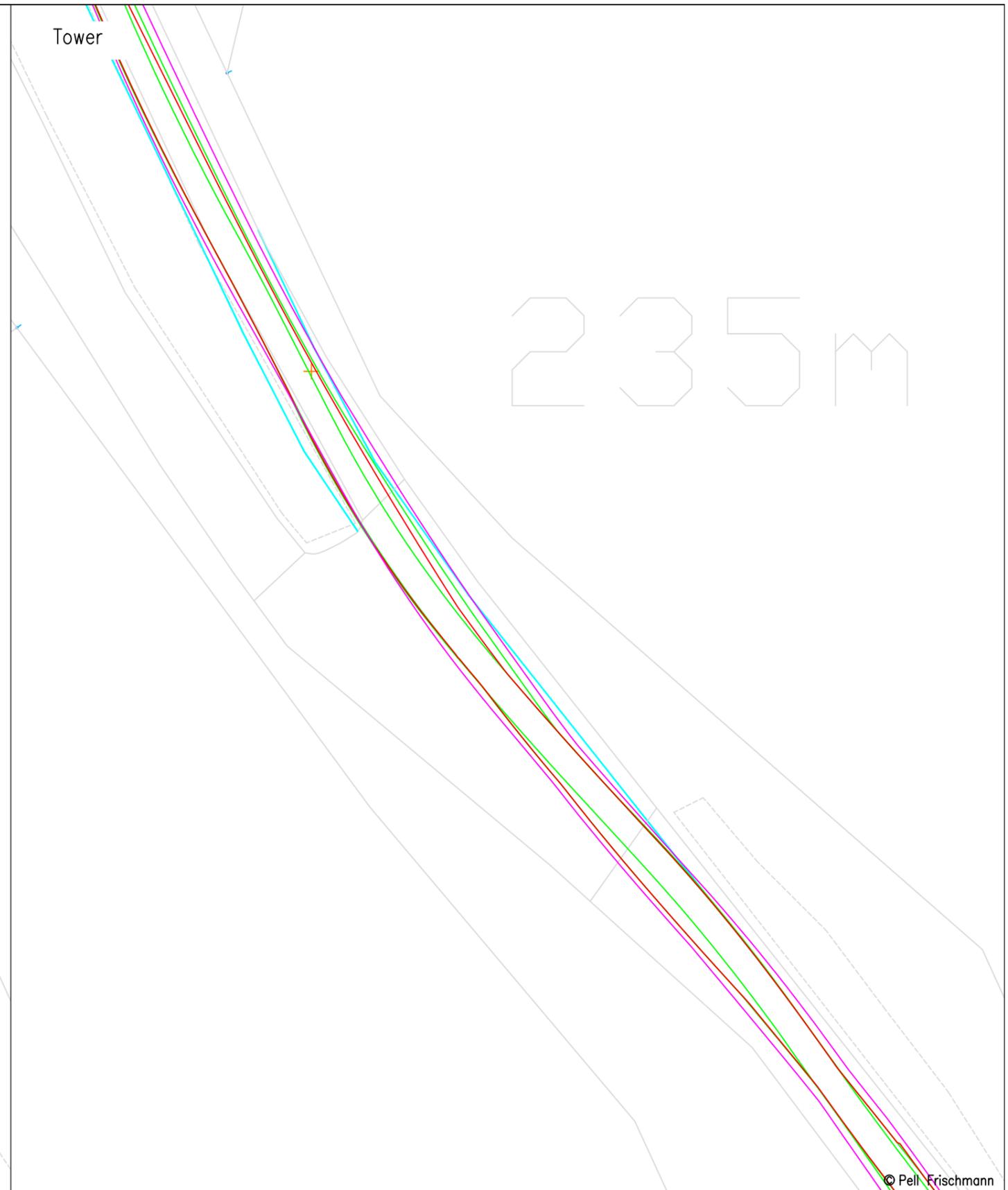
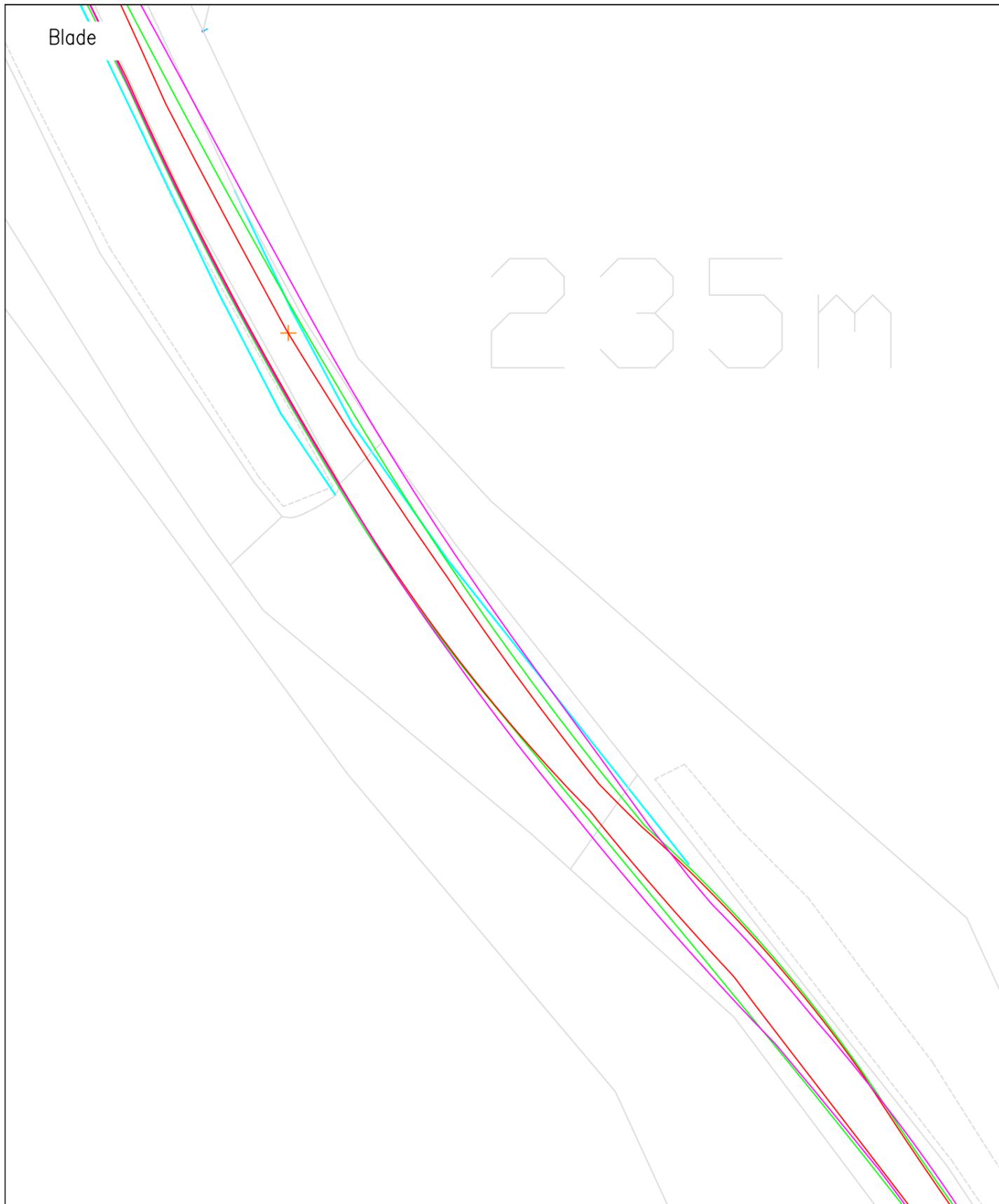
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower		Designed	GB	26/09/2023	File No.
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 Bends at Adamsholm	Checked	GB	26/09/2023	Drawing Status	Draft		
				Point of Interest	15		Drawing No.	SK15	Notes:
								1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 Bends at Adamsholm	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest	15	Drawing No.	SK15A	Notes:	Revision

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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701/B719 Junction	Checked	GB	26/09/2023	Drawing Status	Draft		
				Point of Interest	16		Drawing No.	SK16	Revision
			Notes: 1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.						



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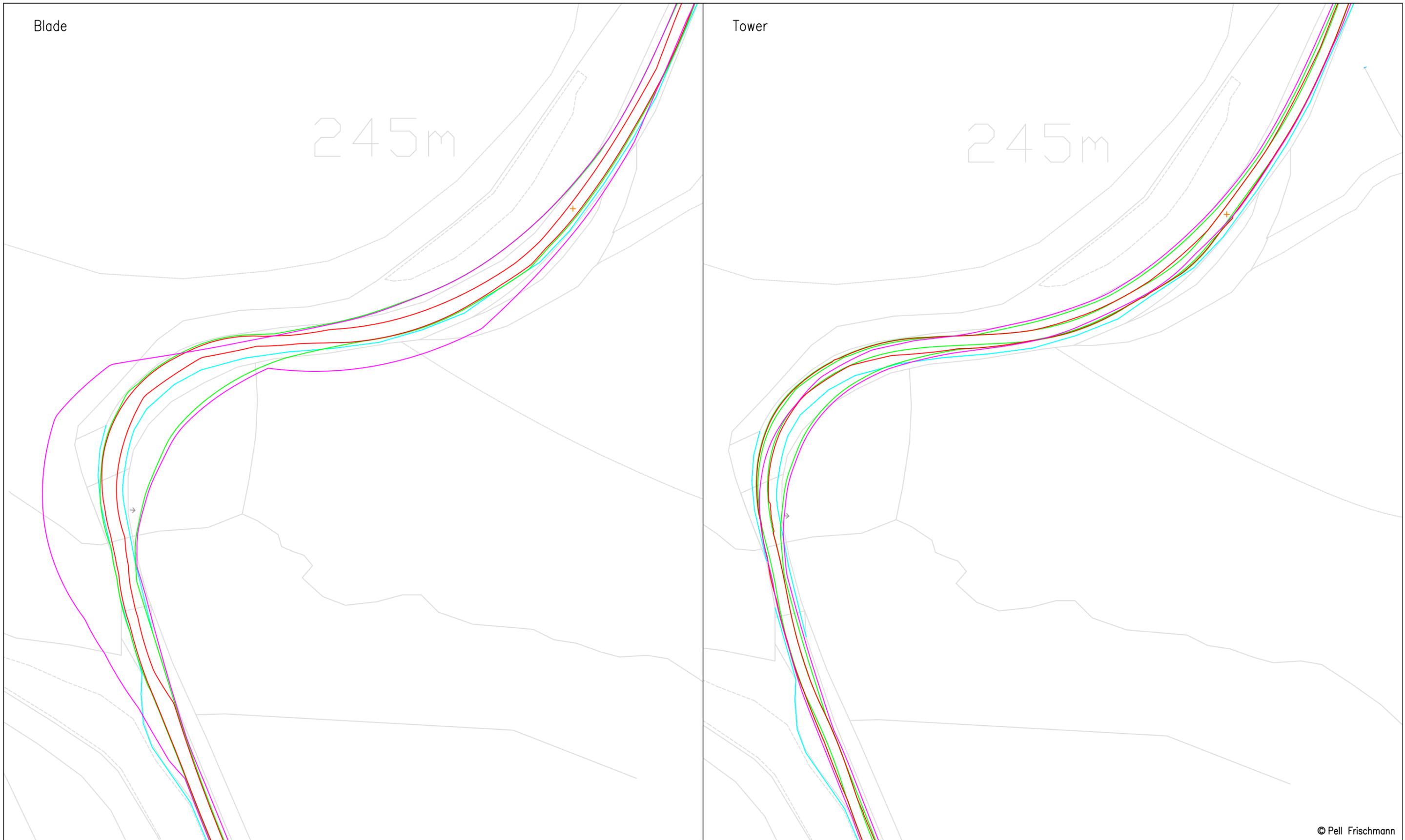
Pell Frischmann <small>93 GEORGE STREET, EDINBURGH, EH2 3ES</small> <small>Tel: +44 (0)131 240 1270</small> <small>Email: pfe@pellfrischmann.com</small> <small>www.pellfrischmann.com</small>	Project	Oliver Forest Wind Farm	Name	AD	Date	26/09/2023	Scale	1:500 @ A3	
	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701/B719 Junction	Checked	GB	26/09/2023	Point of Interest	16	Drawing Status	Draft
			Drawing No.	SK16A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision

Blade

Tower

245m

245m



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Project

Oliver Forest Wind Farm

	Name	Date	Scale
Drawn	AD	26/09/2023	1:1000 @ A3
Designed	GB	26/09/2023	File No. 230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	Drawing Status
Point of Interest		17	Draft

Client Statkraft UK Limited

Drawing Title

Nordex N163 Blade and Tower

Key						
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

SPA Location

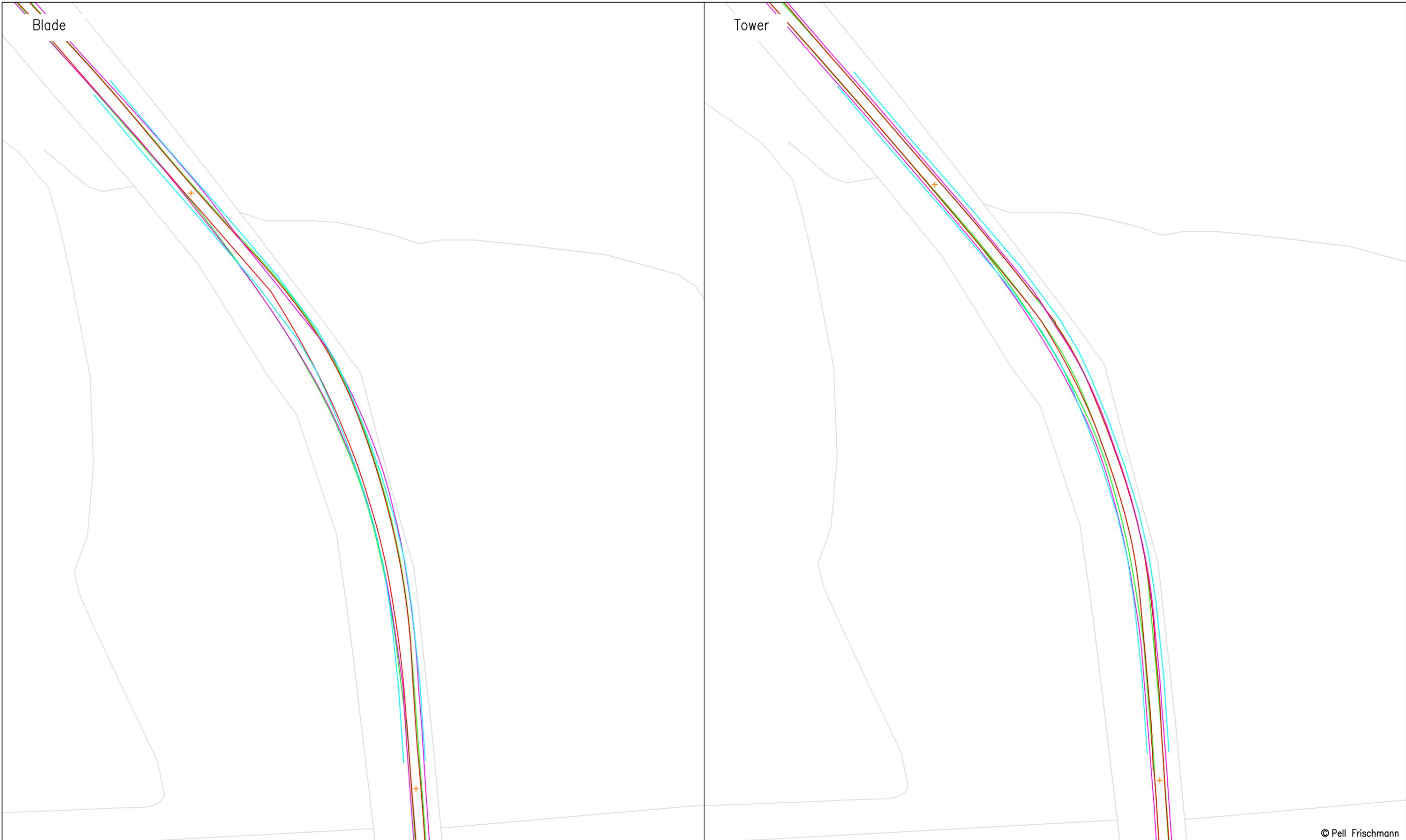
A701 Holehouse Bridge and Bends

Drawing No.	Notes:	Revision
SK17	<ol style="list-style-type: none"> All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 	XXX



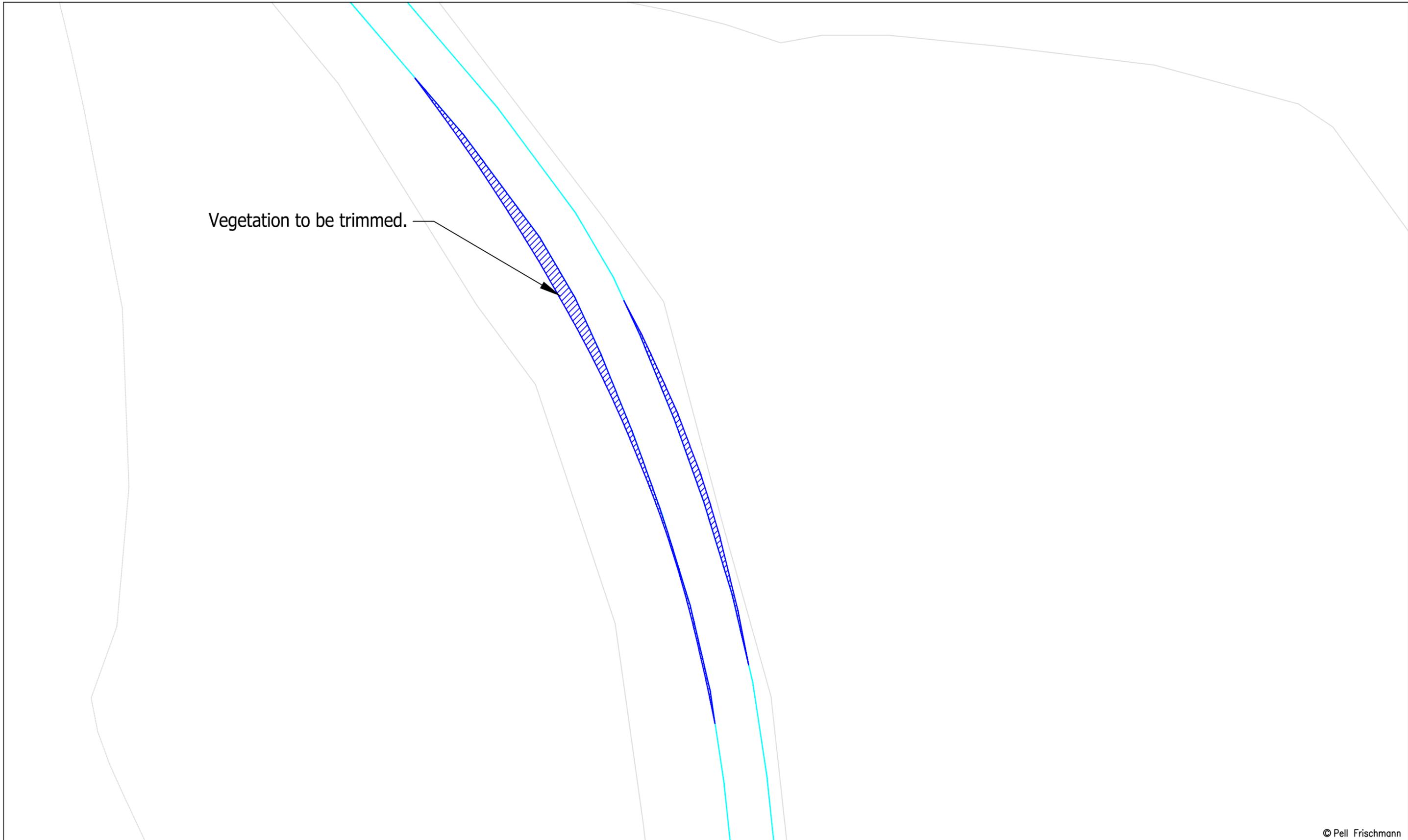
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 Holehouse Bridge and Bends	Checked	GB	26/09/2023	Point of Interest	17	Drawing Status	Draft
			Drawing No.	SK17A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 West of Granton	Checked	GB	26/09/2023	Point of Interest	18	Drawing Status	Draft
			Drawing No.	SK18	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 West of Granton	Checked	GB	26/09/2023	Point of Interest	18	Drawing Status	Draft
			Drawing No.	SK18A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision

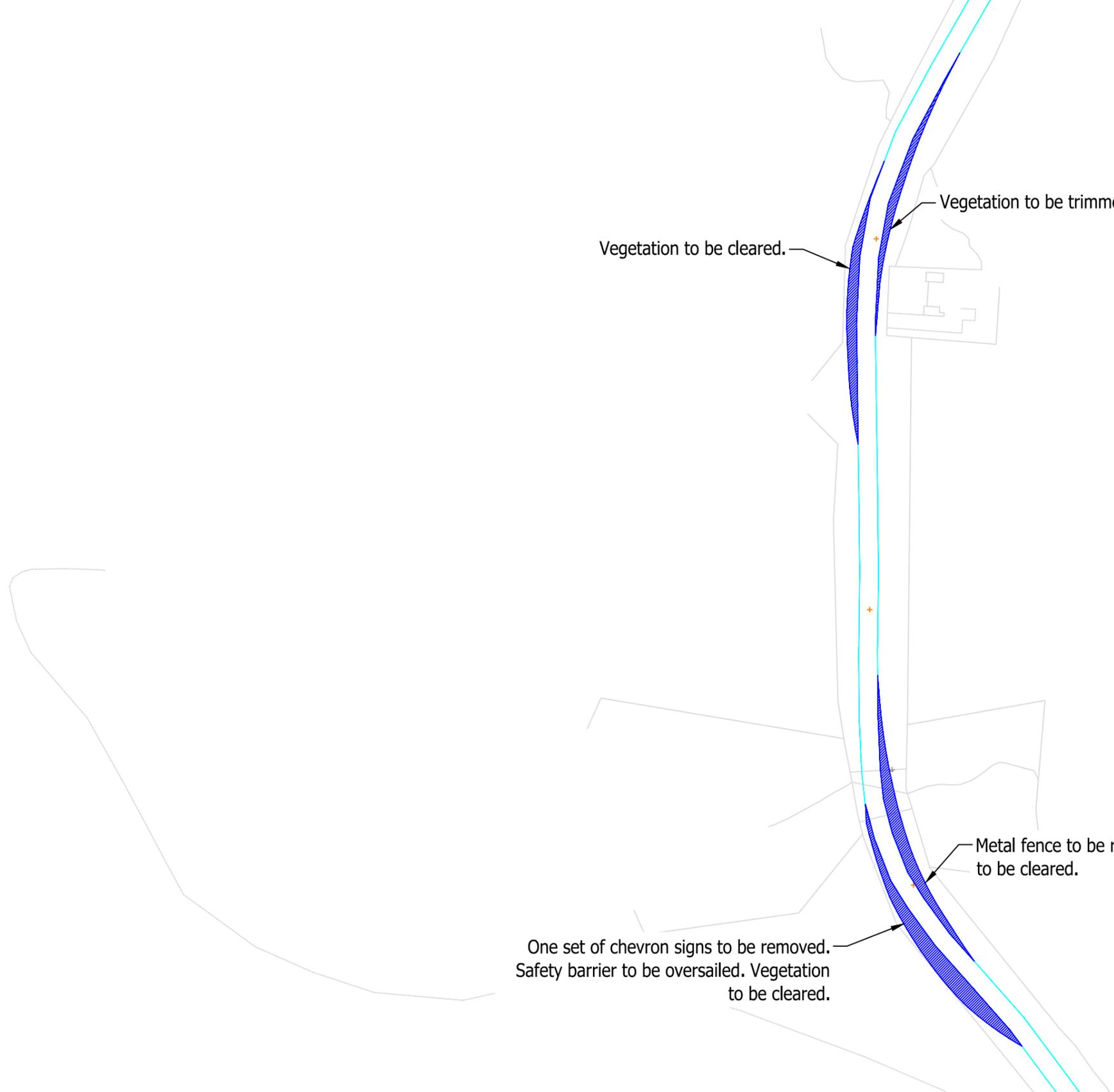
Blade

Tower



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	Client	Statkraft UK Limited	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft
	SPA Location	A701 Aldhousehill Bridge	Point of Interest	20 & 21		Drawing No.	SK19
						Notes:	Revision
						1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 Auldhousehill Bridge	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest	20 & 21		Drawing No.	SK19A	Notes:
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX



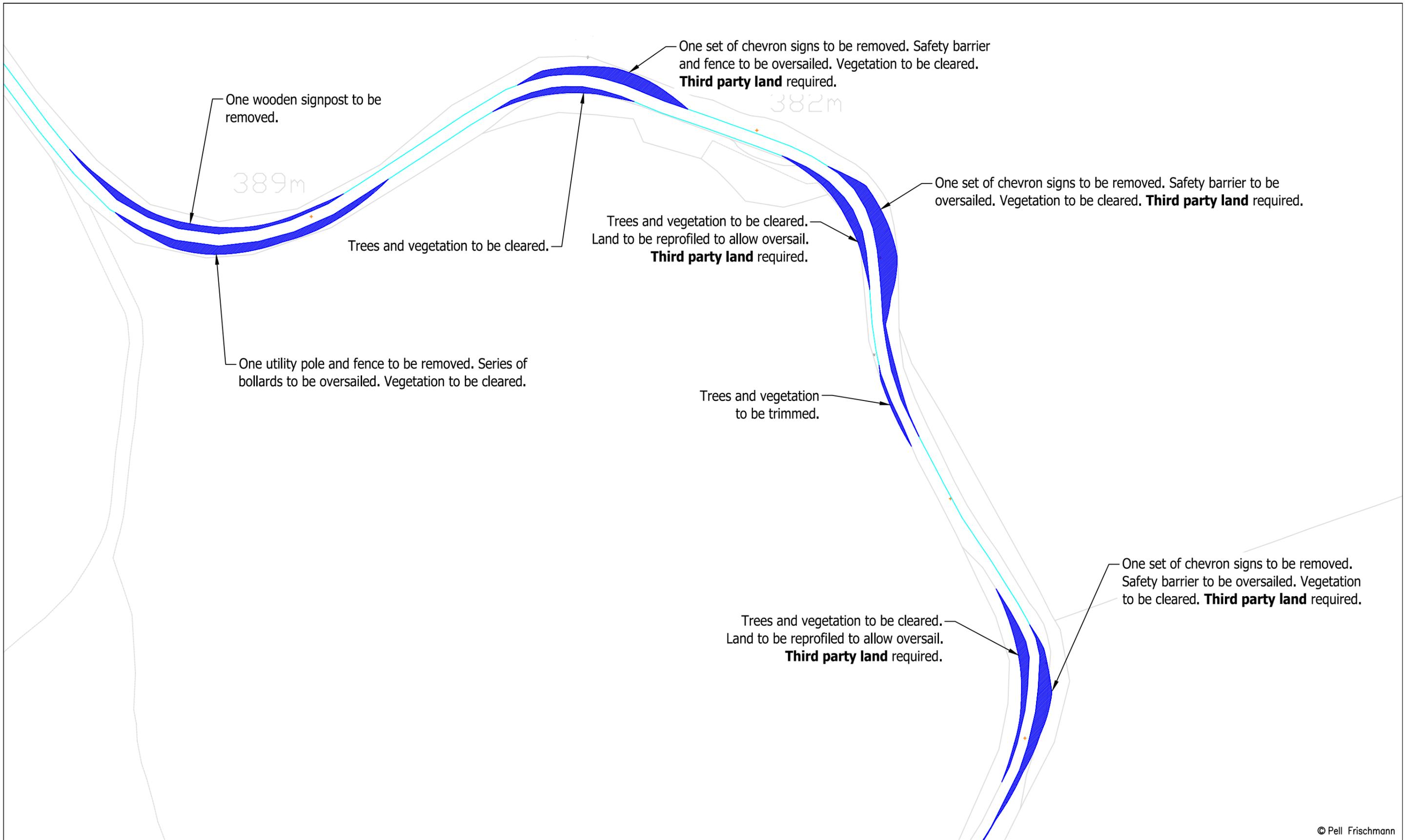
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 Ericstane Bends	Checked	GB	26/09/2023	Drawing Status	Draft	
			Point of Interest	22-25		Drawing No.	SK20	Notes:
							1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 Ericstane Bends	Checked	GB	26/09/2023	Drawing Status	Draft		
				Point of Interest	22-25		Drawing No.	SK20A	Notes:
								1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

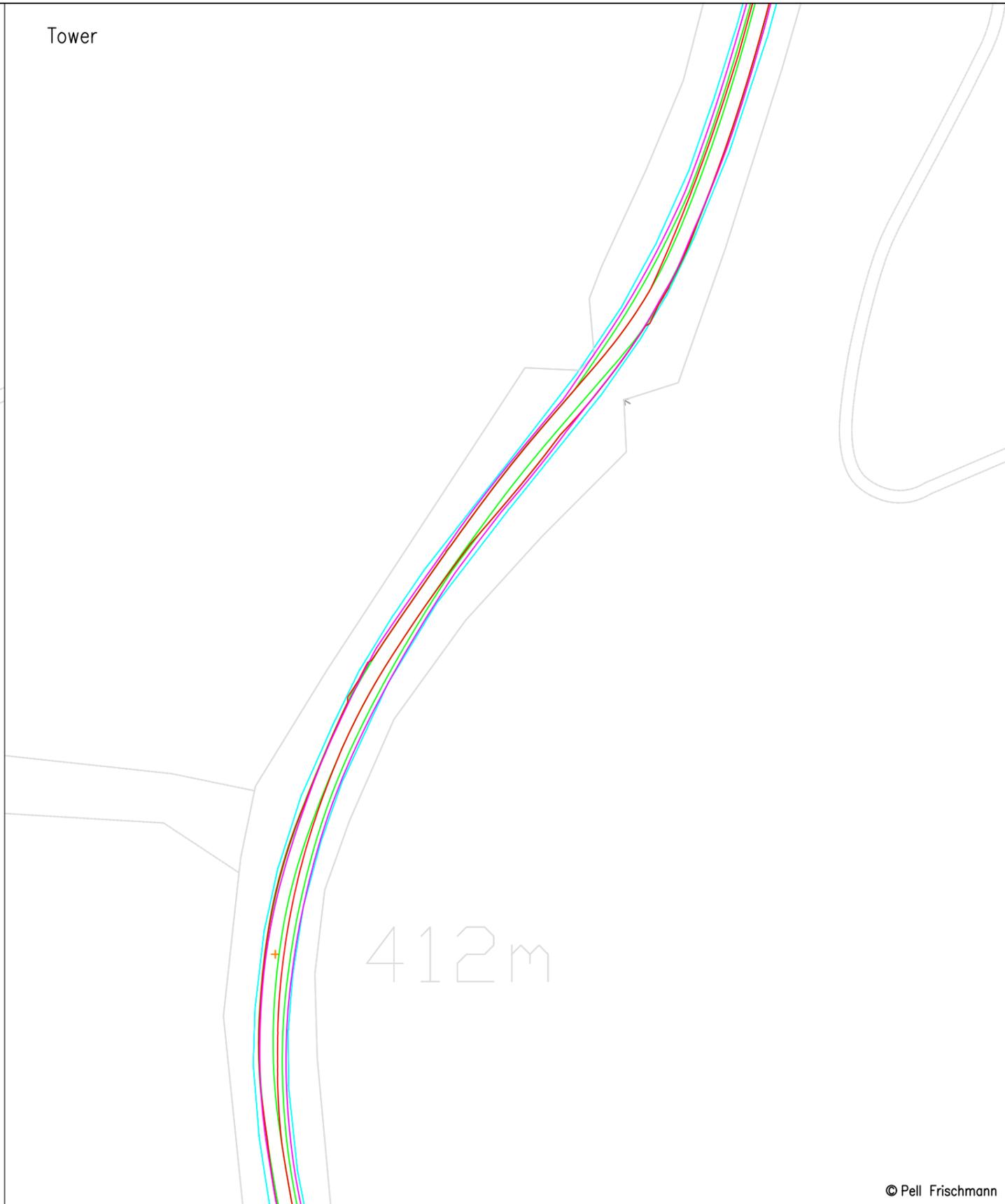


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 Ericstane Bends	Checked	GB	26/09/2023	Drawing Status	Draft	Revision	XXX
			Point of Interest		22-25	Drawing No.	SK20B	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.

Blade

Tower



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Project

Oliver Forest Wind Farm

Drawn	AD	26/09/2023	Scale	1:1000 @ A3
Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	Drawing Status	Draft
Point of Interest	26			

Client **Statkraft UK Limited**

Drawing Title

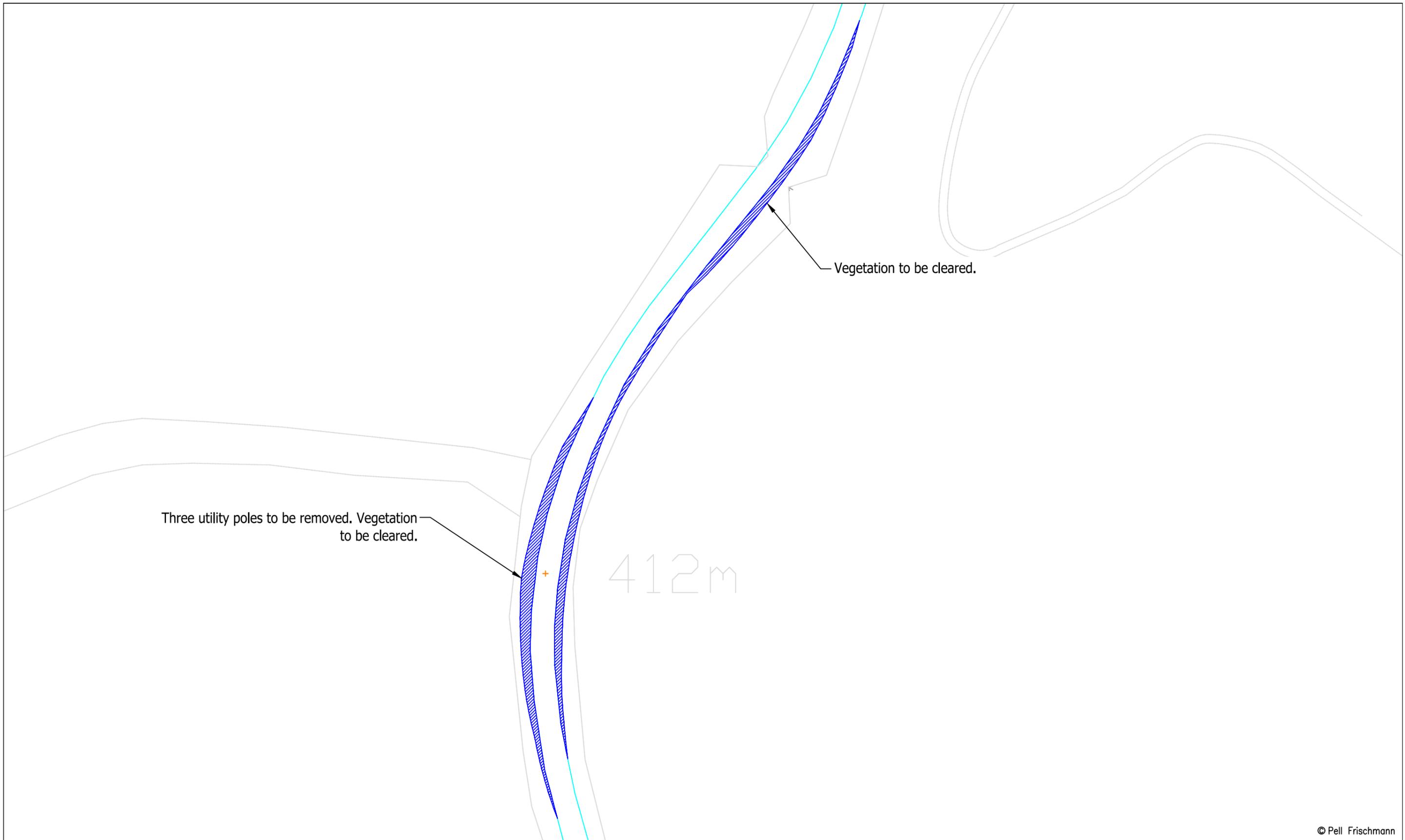
Nordex N163 Blade and Tower

Drawing No.	SK21	Notes:	Revision
		1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

Key						
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

SPA Location

A701 Old Road Bends

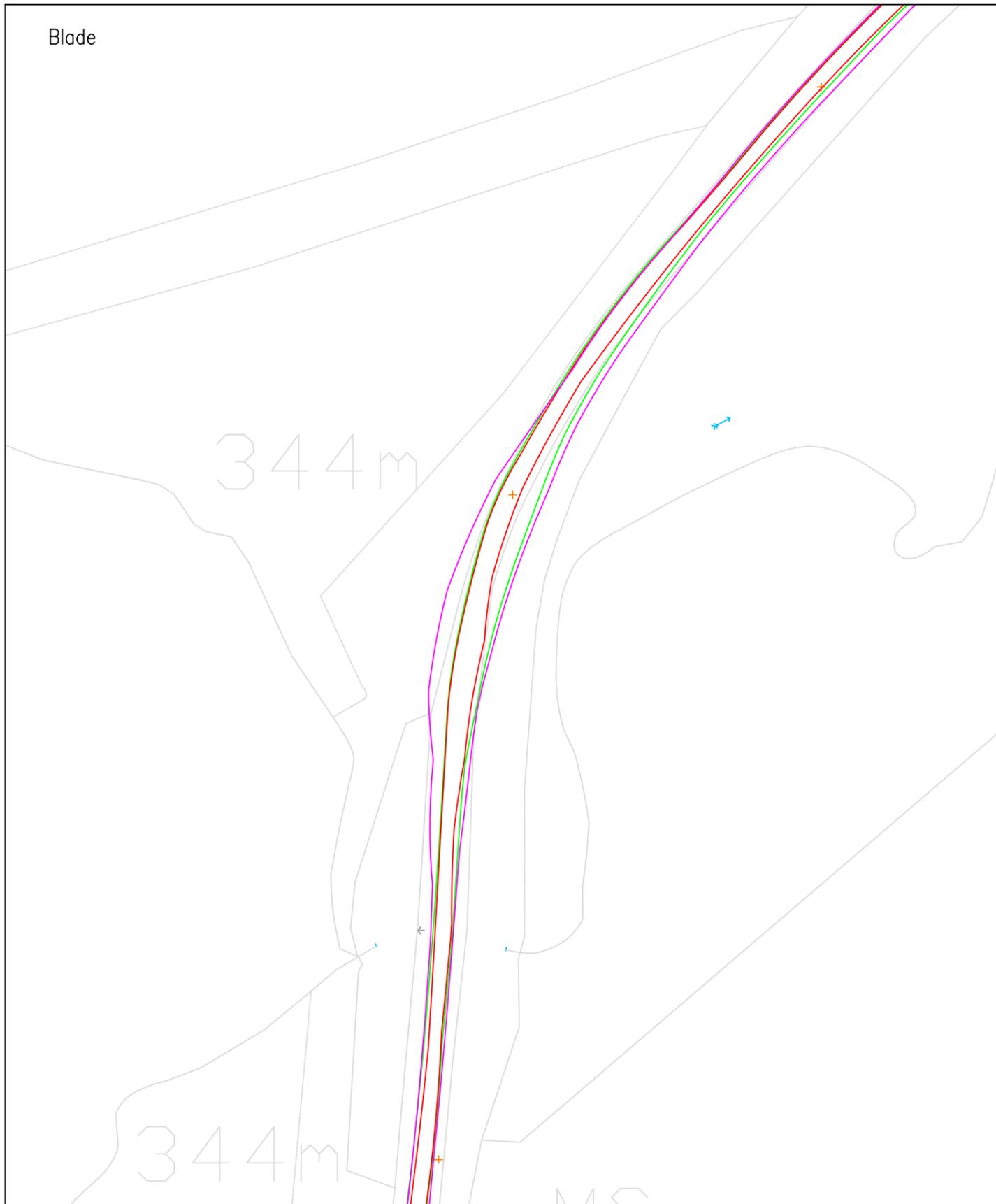


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 Old Road Bends	Checked	GB	26/09/2023	Point of Interest	26	Drawing Status	Draft
			Drawing No.	SK21A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision

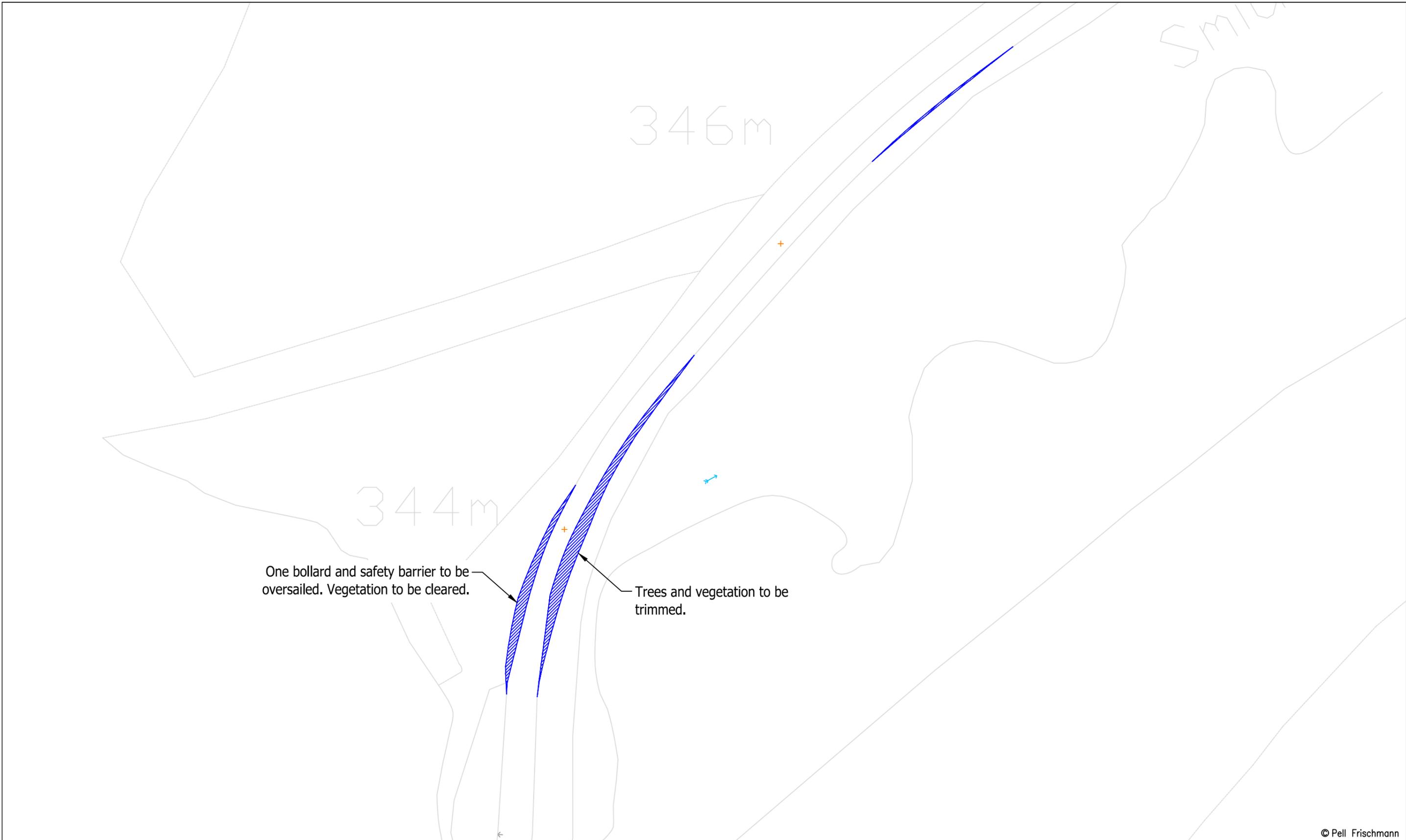
Blade

Tower



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	Client	Statkraft UK Limited			Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg	
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower			Checked	GB	26/09/2023	Drawing Status	Draft	
	SPA Location	A701 Smid Hope Burn			Point of Interest	27		Drawing No.	SK22	
				Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.				Revision	XXX



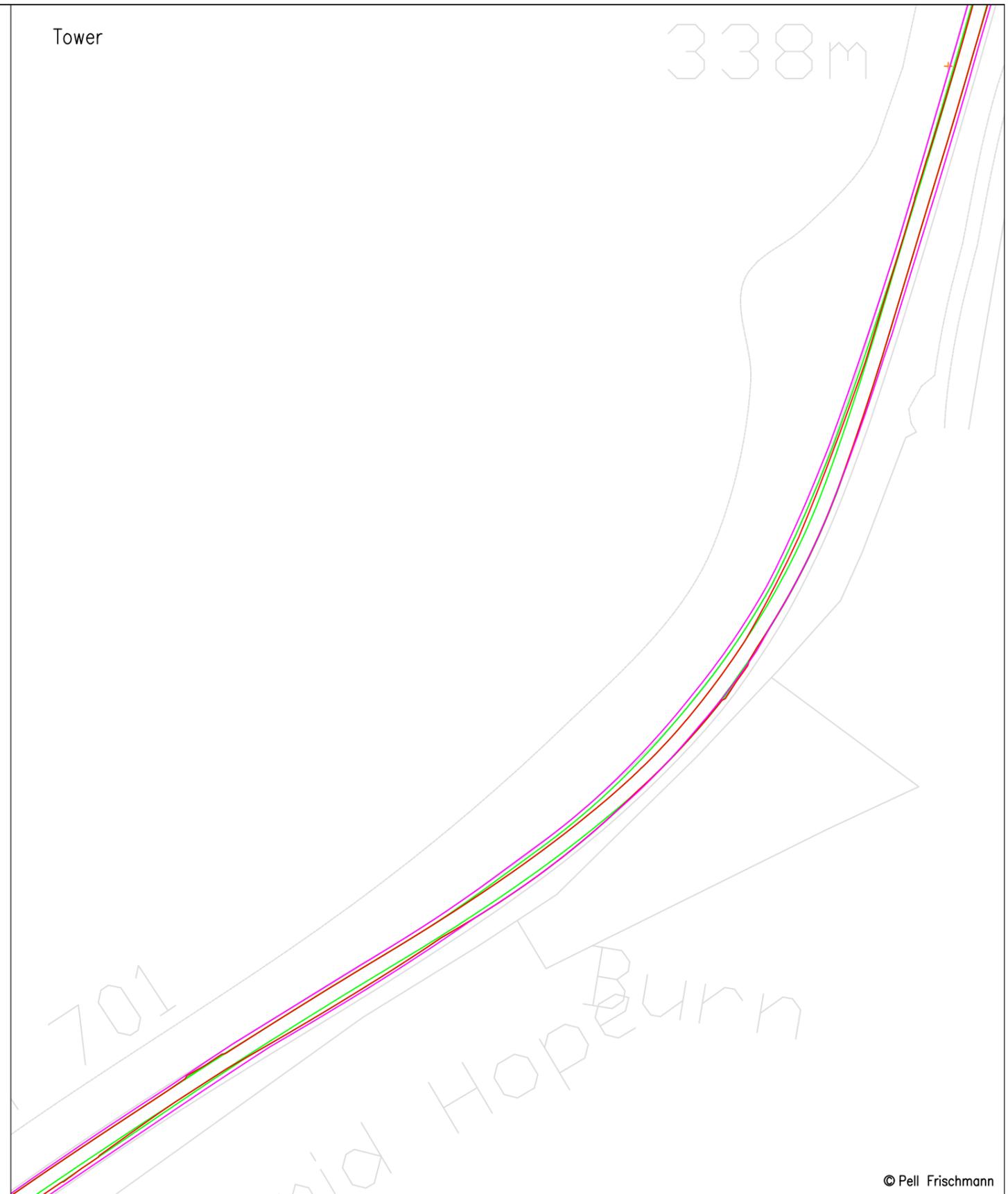
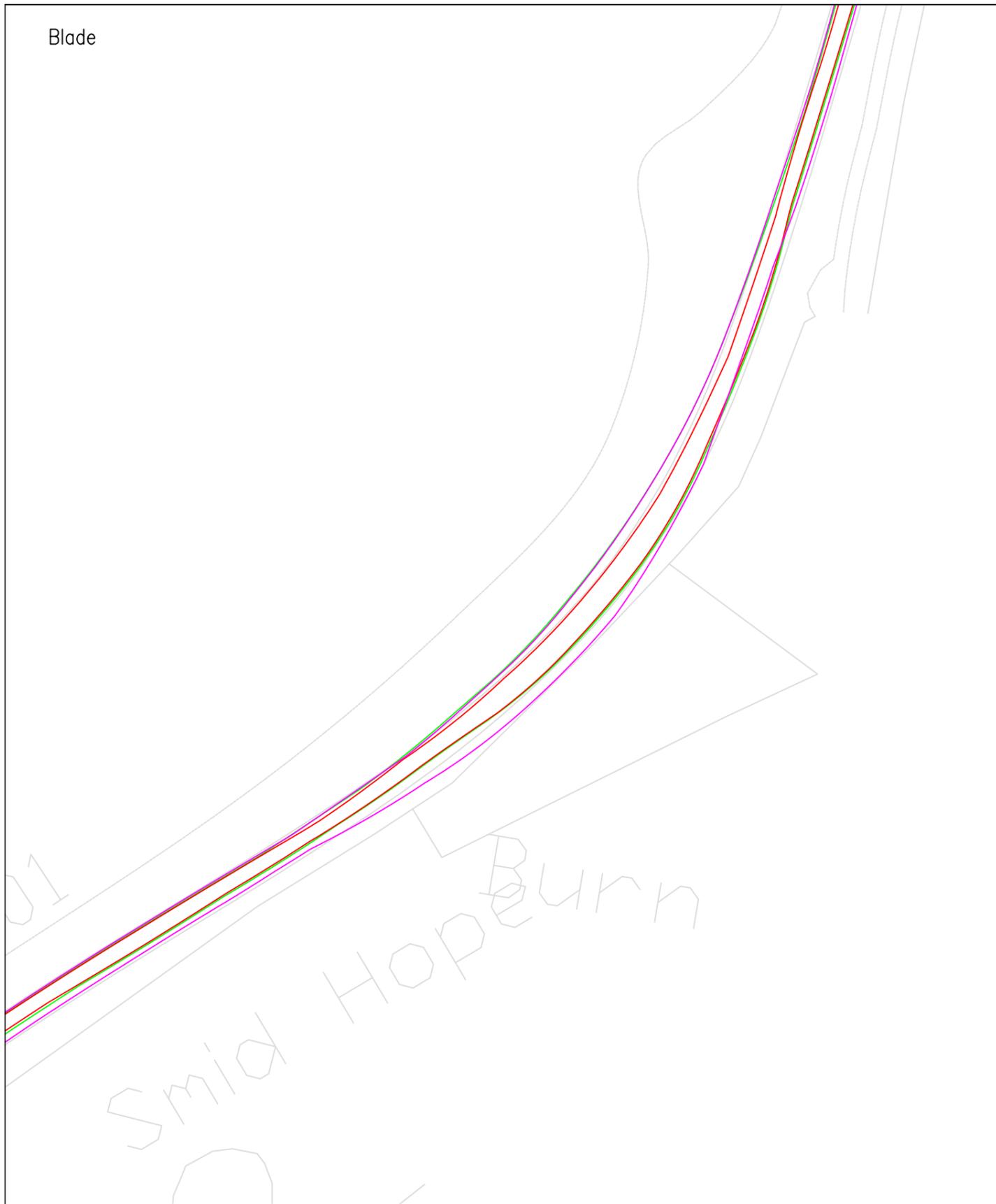
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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 Smid Hope Burn	Checked	GB	26/09/2023	Drawing Status	Draft	Revision	XXX
			Drawing No.	SK22A	Point of Interest	27	Notes:		1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.

Blade

Tower

338m



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Project

Oliver Forest Wind Farm

	Name	Date	Scale
Drawn	AD	26/09/2023	1:1000 @ A3
Designed	GB	26/09/2023	File No. 230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	Drawing Status
Point of Interest		28	Draft

Client Statkraft UK Limited

Drawing Title

Nordex N163 Blade and Tower

Key						
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

SPA Location

A701 Smid Hope Burn

Drawing No.	Notes:	Revision
SK23	<ol style="list-style-type: none"> All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 	XXX

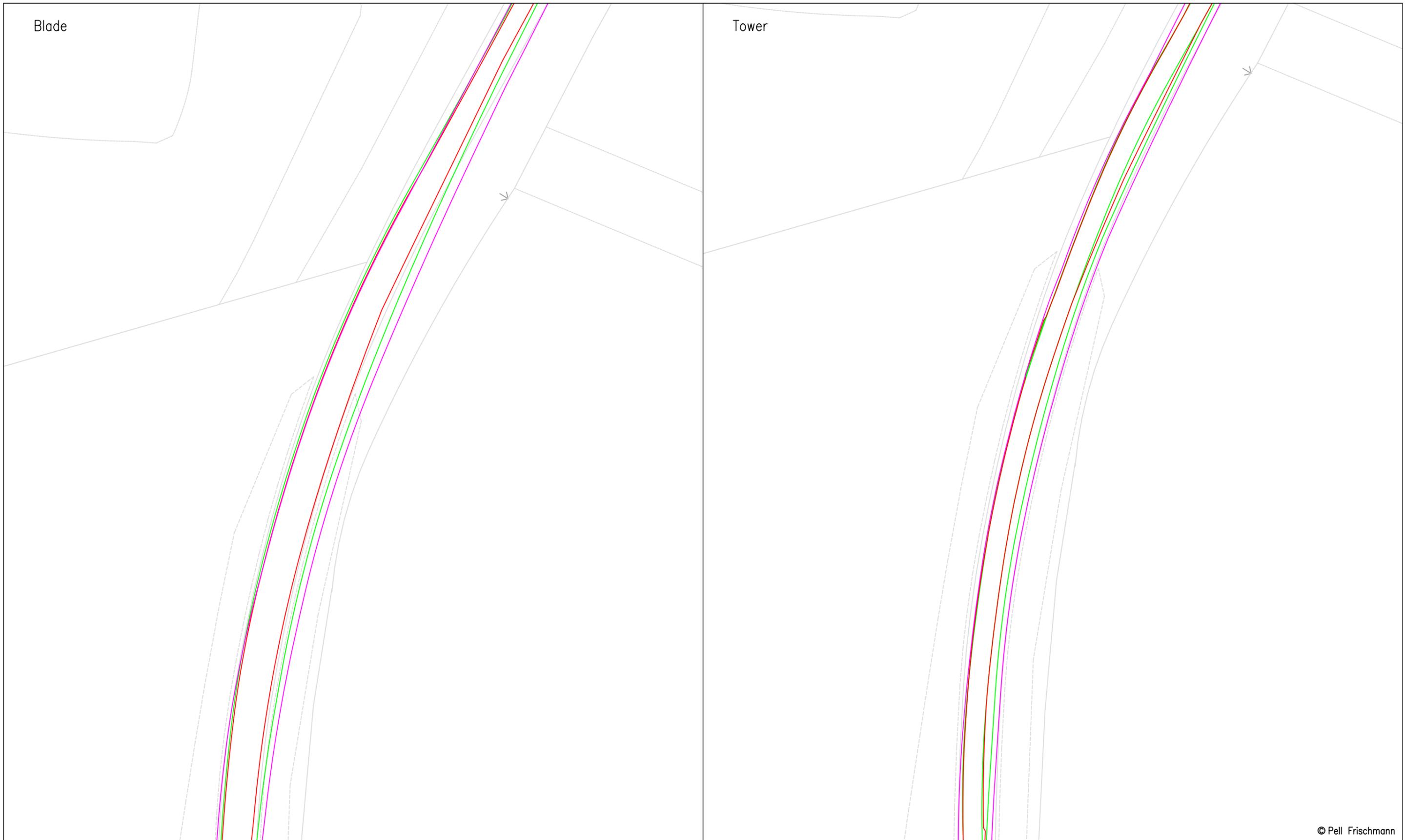


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 Smid Hope Burn	Checked	GB	26/09/2023	Drawing Status	Draft		
			Point of Interest	28		Drawing No.	SK23A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.
							Revision	XXX	

Blade

Tower



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Project

Oliver Forest Wind Farm

	Name	Date	Scale	1:500 @ A3
Drawn	AD	26/09/2023	File No 230912 Oliver Forest SPA N163 FINAL.dwg	Drawing Status Draft
Designed	GB	26/09/2023		
Checked	GB	26/09/2023		
Point of Interest		29	Revision	
Drawing No.	Notes:			XXX
SK24	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			

Client

Statkraft UK Limited

Drawing Title

Nordex N163 Blade and Tower

Key



SPA Location

A701 Fingland



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	Client	Statkraft UK Limited	Drawn	GB	Designed	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	Drawing Title	Nordex N163 Blade and Tower	Checked	GB	26/09/2023	Drawing Status	Draft	
	SPA Location	A701 Fingland	Point of Interest	29		Drawing No.	SK24A	Notes:
							Revision	XXX

1. All mitigation is subject to confirmation through a test run.
 2. This is not a construction drawing and is intended for illustration purposes only.
 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.

Blade

308m

MS

Tower

308m

MS

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Project

Oliver Forest Wind Farm

Drawn	AD	26/09/2023	Scale	1:1000 @ A3
Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Checked	GB	26/09/2023	Drawing Status	Draft
Point of Interest		30		

Client **Statkraft UK Limited**

Drawing Title

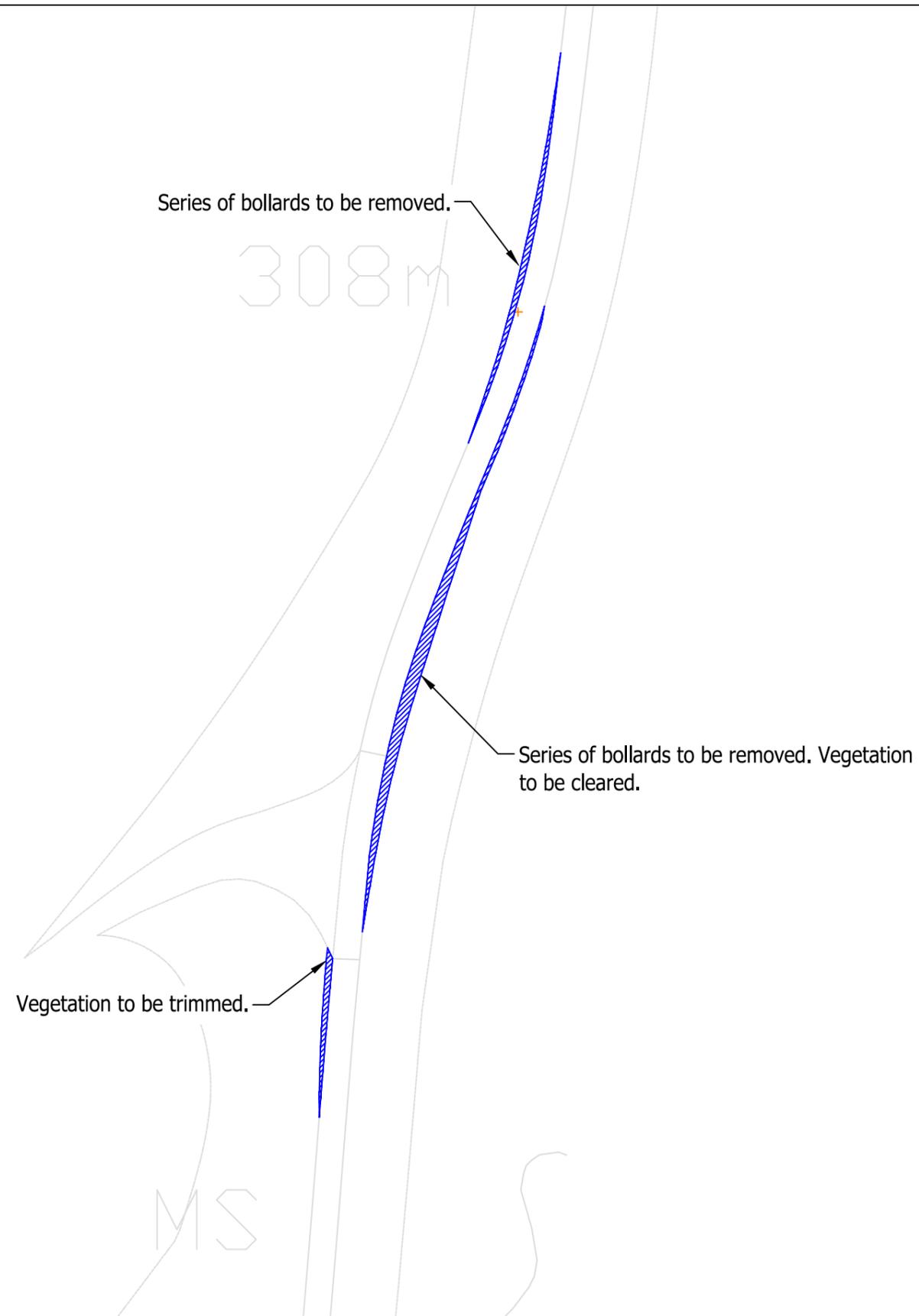
Nordex N163 Blade and Tower

Drawing No.	SK25	Notes:	Revision
		1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	XXX

Key						
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail

SPA Location

A701 East of Risingclaw Heights

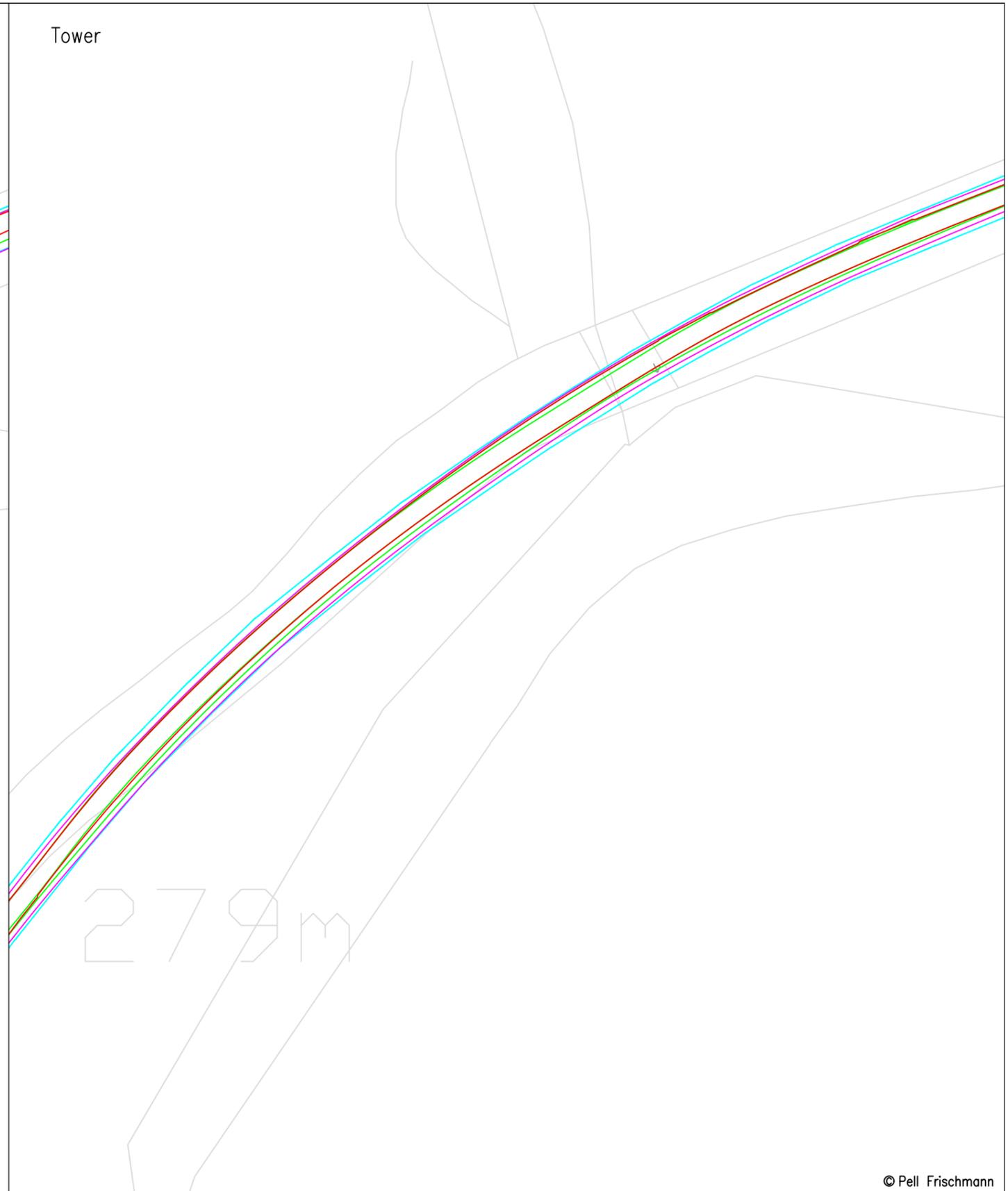
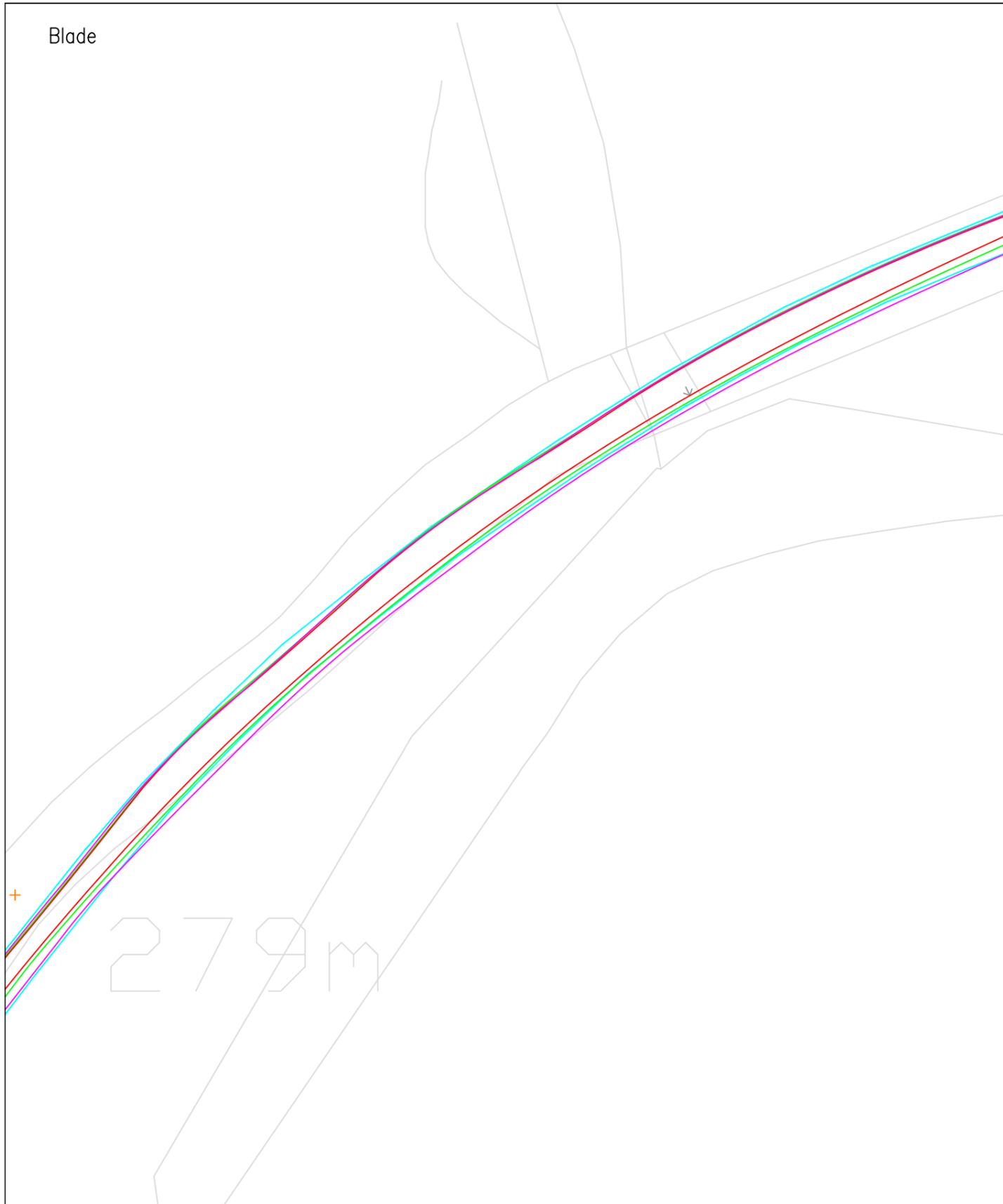


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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 East of Risingclaw Heights	Checked	GB	26/09/2023	Point of Interest	30	Drawing Status	Draft
			Drawing No.	SK25A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision

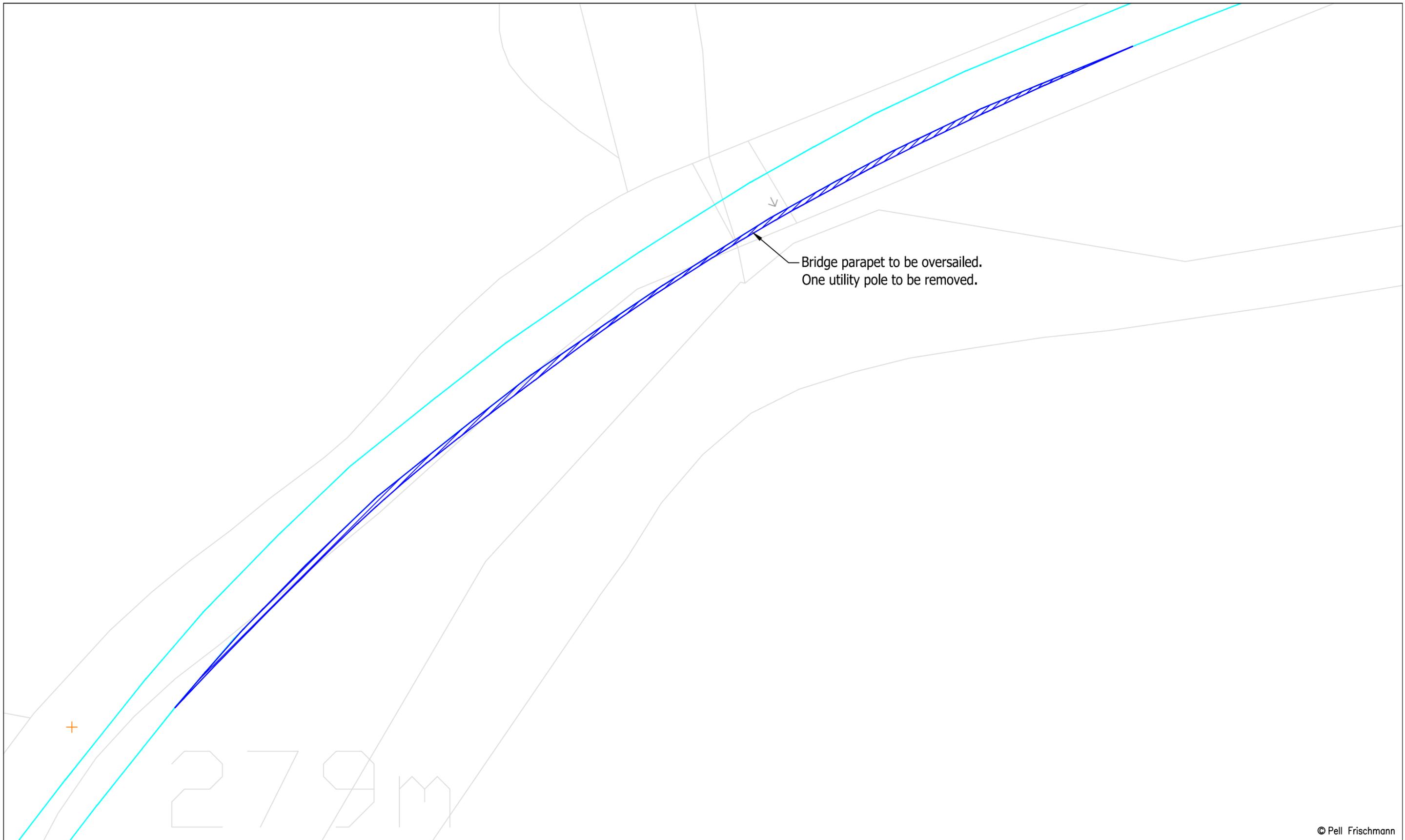
Blade

Tower



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key — Wheel SPA — Body SPA — Load SPA — Indicative Over-run Over-sail	SPA Location	A701 West of Menzion Bend	Checked	GB	26/09/2023	Point of Interest	31	Drawing Status	Draft
			Drawing No.	SK26	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision



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	Client	Statkraft UK Limited	Drawing Title	Nordex N163 Blade and Tower	Designed	GB	26/09/2023	File No.	230912 Oliver Forest SPA N163 FINAL.dwg
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location	A701 West of Menzion Bend	Checked	GB	26/09/2023	Point of Interest	31	Drawing Status	Draft
			Drawing No.	SK26A	Notes:	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.			Revision