Pell Frischmann

Oliver Forest Wind Farm

Route Survey Report

June 2024 107077 This report is to be regarded as confidential to our Client and is intended for their use only and may not be assigned except in accordance with the contract. Consequently, and in accordance with current practice, any liability to any third party in respect of the whole or any part of its contents is hereby expressly excluded, except to the extent that the report has been assigned in accordance with the contract. Before the report or any part of it is reproduced or referred to in any document, circular or statement and before its contents or the contents of any part of it are disclosed orally to any third party, our written approval as to the form and context of such a publication or disclosure must be obtained.

Report Ref.		240619 Oliver Forest Rsr - V2					
File Path		https://pellf.sharepoint.com/sites/EdinburghOfficeTeam/Shared Documents/General/Projects/107077 Statkraft Oliver Forest/01 - WIP/Reports/RSR/240619 Oliver Forest RSR - V2.docx					
Rev	Suit	Description	Date	Originator	Checker	Approver	
А		Draft	25/09/2023	A Dimitrov	T Lockett	G Buchan	
В		Updated with Client Comments	18/06/2024	E Moran	T Lockett	G Buchan	
Ref. reference. Rev revision. Suit suitability.							

Prepared for

Oliver Forest Wind Farm Limited

22 Bishopsgate London EC2N 4BQ

Prepared by

Pell Frischmann

93 George Street Edinburgh EH2 3ES

Contents

1		Introduction	1
	1.1	Purpose of the Report	1
2		Site Background	2
	2.1	Site Location	2
	2.2	Candidate Turbines	2
	2.3	Proposed Delivery Equipment	3
3		Access Route Review	4
	3.1	Port of Entry	4
	3.2	Proposed Access Route	4
	3.3	Route Constraints	5
	3.4	Swept Path Assessment Results and Summary	14
	3.5	Land Ownership	14
	3.6	Summary Issues	14
4		Summary1	5
	4.1	Summary of Access Review	15
	4.2	Further Actions	15

Figures

Figure 2-1: Site Location Plan	2
Figure 2-2: Super Wing Carrier Trailer	3
Figure 3-1: Proposed Access Route	4

Tables

Table 2-1: N163 Turbine Size Summary	2
Table 3-1: Constraint Points and Details	5

Appendices

Appendix A Points of Interest Locations Appendix B Swept Path Assessments

1 Introduction

1.1 Purpose of the Report

Pell Frischmann (PF) has been commissioned by Oliver Forest Wind Farm Limited (the Applicant) to undertake a survey of the delivery route for wind turbine Abnormal Indivisible Loads (AIL) associated with the construction and development of Oliver Forest Wind Farm, located in the Tweed Valley in the Southern Uplands of Scotland.

The Route Survey Report (RSR) has been prepared to help inform the Applicant on the likely issues associated with the development of the site with regards to on-site transport and access for AIL traffic. The report identifies the key issues associated with AIL deliveries and notes that remedial works, either in the form of physical works or as traffic management interventions, will be required to accommodate the predicted loads.

The detailed assessment and subsequent designs of any remedial works are beyond the agreed scope of works between PF and the Applicant at this point in time.

It is the responsibility of the wind turbine supplier to ensure that the entirety of the proposed access route is suitable and meets with their satisfaction. The turbine supplier will be responsible for ensuring that the finalised proposals meet with the appropriate levels of health and safety consideration for all road users and has been made in accordance with the relevant legislation at the time of delivery.

2 Site Background

2.1 Site Location

The Proposed Development site is located in the Tweed Valley in the Southern Uplands of Scotland, spread across the extent of three named hills: Upper Oliver Dod, Weird Law, and Glenmuck Height. Figure 2-1 illustrates the general site location.

Figure 2-1: Site Location Plan



2.2 Candidate Turbines

The Applicant has indicated that they wish to consider the use of a Nordex N163 turbine at a tip height of 200m.

The details of the components are summarised in Table 2-1.

Component	Length (m)	Width (m)	Height (m)	Weight (t)
Blade	81.5	4.395	4.110	28.871
Base Tower	11.561	4.300	4.292	83.318
Mid Tower 1	16.430	4.292	4.286	82.391
Mid Tower 2	21.125	4.286	4.279	81.401
Mid Tower 3	29.972	4.279	4.268	82.563
Top Tower	35.000	4.268	3.258	60.569

Table 2-1: N163 Turbine Size Summary

2.3 Proposed Delivery Equipment

To provide a robust assessment scenario based upon the known issues along the access route, it has been assumed that all blades would be carried on a Super Wing Carrier trailer.

Towers would be carried in a 4+7 clamp adaptor style trailer, whereas loads such as the hub, nacelle housing, and top towers would be carried on a six-axle step frame trailer.

Figure 2-2: Super Wing Carrier Trailer



Figure 2-3: Tower Trailer



3 Access Route Review

3.1 Port of Entry

The proposed Port of Entry (POE) is KGV Docks in Glasgow. The port is the closest suitable port to site and as such is in line with the Government's "Water Preferred" policy¹ towards AIL movements.

The port has been used by renewables deliveries in the past for a number of wind farms, including Kype Muir, Kilgallioch, and Clyde wind farms.

The port has sufficient quay and storage space and is well located for the strategic trunk road network.

3.2 Proposed Access Route

The proposed access route from KGV Docks to the site access junction is as follows:

- Loads would exit KGV Docks in Glasgow onto Kings Inch Drive;
- Loads would continue along Kings Inch Drive before turning left onto the M8 slip road, Mayo Avenue;
- Loads would then merge onto the M8 at junction 25A;
- Vehicles would continue east on the M8 to Junction 21 where they would join the M74 travelling south;
- Loads would depart the A74(M) at Junction 15;
- Loads would turn left at the Junction 15 roundabout and would join the A701 northbound, passing through Moffat;
- Vehicles would continue on the A701 northbound;
- To the south of Tweedsmuir, loads would turn left into an upgraded access junction and would continue to site using private access tracks.

The proposed access route is illustrated in Figure 3-1.

Figure 3-1: Proposed Access Route



¹ Highways England (2019), Water Preferred Policy – Guidelines for the movement of abnormal indivisible loads

3.3 Route Constraints

The constraints noted on the route are detailed in Table 3-1. No consideration of the transport issues within the Proposed Development site have been undertaken.

Plans illustrating the location of the constraints are provided in Appendix A.



POI	Key Constraint	Details
1	Kings Inch Drive Roundabout 1	Loads will exit the port driving through the roundabout onto Kings Inch Drive heading northwest.
		Loads will overrun and oversail the central island of the roundabout where the existing overrun area should be utilised.
		Loads will also oversail the central reservation of the exit arm of the roundabout where two road signs should be removed.
		Loads will oversail the western verge of the exit arm.
		Swept path drawing SK01 is included in Appendix B.
2	Kings Inch Drive Roundabout 2	Loads will proceed ahead at the roundabout, taking the second exit.
		Loads will oversail both verges of the entry arm on approach. One lighting column should be removed from the southern verge.
		Loads will oversail the southern verge of the roundabout island where two road signs to be removed.
	and a did see a little	Vehicles will oversail the southern verge of the exit arm.
		Swept path drawing SK02 is included in Appendix B.
3	Kings Inch Drive Roundabout 3	Loads will take the second exit at the roundabout.
	A	Loads will oversail the southern verge of the roundabout where a load bearing surface should be laid
		Swept path drawing SK03 is included in Appendix B.
4	Kings Inch Drive / Mayo Avenue Junction	Loads will turn left at the junction joining the M8 slip road.
		Loads will oversail the central reserve on approach where escorts are required to hold oncoming vehicles during movements. One lighting column, one traffic signal and a section of pedestrian guardrail should be removed. Loads will oversail the northern footway of the entry arm.
	Contraction in the local	Loads will oversail the splitter island where three traffic signals, one pedestrian call post and a section of guardrail should be removed. One bollard will be oversailed.
		Loads will oversail the inside of the junction where one pedestrian call post and one road sign should be removed. Trees and vegetation should be trimmed.
		Swept path drawing SK04 is included in Appendix B.

POI	Key Constraint	Details
5	Mayo Avenue Bend Leading to M8 Junction 25A	Loads will pass under the A8 on the slip road before joining the M8 headed southeast.
		After passing the bridge loads will oversail a safety barrier on the left-hand side of the road.
		Swept path drawing SK05 is included in Appendix B.
6	A74(M) Junction 15 / A701 Roundabout	Loads will depart the A74(M) at Junction 15 and join the A701 heading northeast. When exiting the A74(M) via the slip lane and approaching Junction 15, escorts should ensure that trailing traffic does not try to merge into the convoy.
		The blade tip will oversail the right-hand verge of the entry arm where one lighting column, one road sign and fence should be removed. A section of safety barrier and one junction box will be oversailed. Trees and vegetation should be cleared. Land reprofiling may be required to allow the blade tip to oversail; therefore, it is highly recommended that the swept path assessment is repeated on a topographical base survey to confirm the required mitigation.
		Loads will overrun and oversail the northern verge of the entry arm where a load bearing surface should be laid. One road sign, one lighting column and a section of fence should be removed. Trees and vegetation should be removed. Land should be reprofiled to allow oversail.
		Vehicles will overrun and oversail the northern verge of the roundabout island where a load bearing surface should be laid and one set of chevron signs should be removed.
		Vehicles will oversail the exit splitter island where two bollards should be removed. The vehicles will overrun and oversail the eastern verge of the exit arm where a load bearing surface should be laid.
		Swept path drawing SK06 is included in Appendix B.
7	A701 Entering Moffat	Loads will continue north through two bends towards Moffat.
		Loads will oversail both verges through the section. There is minimal clearance to structures to the east of the road and it is recommended that a topographical survey is utilised to confirm the feasibility of the manoeuvre.
		Loads will overrun and oversail the western verge through the second bend where a load bearing surface should be laid. One utility pole should be removed and vegetation should be trimmed.
		Swept path drawing SK07 is included in Appendix B.



POI	Key Constraint	Details
10	A701 High Street and Academy Road Junction	Loads will remain on the A701 exiting Moffat through a left bend with a mini roundabout. Loads will keep left, taking the first exit at the roundabout.
		On approach to the bend loads will oversail both verges of Harthope Place, an eastbound side road, where one lighting column, two sign posts, two decorative planters and a section of pedestrian guardrail should be removed.
		On the southwestern verge through the bend, loads will oversail the footway where one road sign should be removed.
		Loads will overrun and oversail the northeastern verge through the bend as they approach the roundabout. A load bearing surface should be laid. One lighting column, one junction box, a section of pedestrian guardrail and vegetation should be removed.
		At the mini roundabout, loads will overrun and oversail the splitter islands of all three arms where load bearing surfaces should be laid and five bollards should be removed.
		Loads will overrun and oversail the northern verge of the exit arm where a load bearing surface should be laid. One lighting column and one road sign should be removed.
		Swept path drawing SK10 is included in Appendix B.
11	A701 South of Moffat Cemetery	Loads will head northwest on the A701 towards Moffat Cemetery through a right bend.
	ALL ALL	Loads will oversail the northern verge of the A701 through the bend.
		Two traffic islands should be cleared of street furniture. Throughout the route, the tree canopy needs to be trimmed to provide a clear 5m head height. Trimming of the tree canopy can be subject to ecological constraints and it is suggested that early consultation with the local road authority is undertaken to agree cutting times and permits.
		Swept path drawing SK11 is included in Appendix B.
12	A701 Moffat Cemetery	Loads will drive past Moffat Cemetery through a series of bends.
		Approaching the first bend, the blade tip will oversail the southern verge of the A701 where trees and vegetation should be removed. Loads will overrun and oversail the eastern verge where a load bearing surface should be laid. One utility pole should be removed. One bollard and a section of fence will be oversailed. Third party land is required.
		Loads will oversail a series of bollards on the western verge through the second and third bend. One gate and fence should be removed. Trees and vegetation should be cleared. Third party land is required.
		Loads will oversail a series of bollards on the eastern verge through the third bend. Trees and vegetation should be cleared. Third party land is required.
		Swept path drawing SK12 is included in Appendix B.

POI	Key Constraint	Details
13	A701 North of Moffat Cemetery	Loads will continue north on the A701.
		Loads will oversail both verges. On the western verge, a series of bollards will be oversailed and vegetation should be trimmed at two locations. On the eastern verge, one road sign and tree should be removed.
		Swept path drawing SK13 is included in Appendix B.
14	A701 Bends South of Adamsholm	Loads will continue on the A701 northbound through two bends.
		Loads will oversail both verges through both bends. On the western verge, a wall section should be removed from each bend where there is oversail. Trees and vegetation should be cleared. Land reprofiling is required at both bends to allow the blade tip to oversail. On the eastern verge, vegetation should be trimmed.
		Swept path assessment SK14 is included in Appendix B.
15	A701 Bends at Adamsholm	Loads will remain on the A701 heading north through two bends.
		Loads will oversail a series of bollards on the eastern verge through the first bend where three sets of chevron signs should be removed. Trees and vegetation should be cleared.
		Loads will oversail the western verge through both bends. Trees and vegetation should be cleared.
		Loads will oversail the eastern verge through the second bend. One bollard will be oversailed. Trees and vegetation should be cleared.
		Throughout the route, the tree canopy needs to be trimmed to provide a clear 5m head height. Trimming of the tree canopy can be subject to ecological constraints and it is suggested that early consultation with the local road authority is undertaken to agree cutting times and permits.
		Swept path drawing SK15 is included in Appendix B.
16	A701/B719 Junction	Loads will remain on the A701 heading north.
	and the same	Loads will oversail the eastern verge passing through the junction. One road sign should be removed and vegetation should be trimmed.
	and the second and the	Swept path drawing SK16 is included in Appendix B.

POI	Key Constraint	Details
17	A701 Holehouse Bridge and Bends	Loads will continue north through a right/left bend section.
		Indicative road edges have been provided for illustration only due to the lack of detail on available OS base mapping. Mitigation measures should be confirmed through the test run.
		Loads will oversail both verges through the initial right bend where third party land is required on both sides of the road. The loads should be raised using vehicle suspension to enable oversail of the bridge parapet and safety barrier on the inside of the bend where trees and vegetation should be cleared. Loads will oversail the bridge parapet and safety barrier on the outside of the bend where two sets of chevron signs should be removed and trees and vegetation should be removed.
		Loads will oversail both sides of the left bend section. On the outside of the left bend, one road sign should be removed and a safety barrier will be oversailed. On the inside, a series of bollards will be oversailed and vegetation should be trimmed.
		Swept path assessment SK17 is included in Appendix B.
18	A701 West of Granton	Loads will remain on the A701 heading northwest through a left bend.
	and the second se	Loads will oversail both verges. On the left verge, vegetation should be trimmed.
		Swept path drawing SK18 is included in Appendix B.
19	A701 East of Archie's Hill	Loads will remain on the A701 heading north through a right bend where loads will oversail the eastern verge.
20,	A701 Auldhousehill Bridge	Loads will continue northbound on the A701 through two right
21		bends. Through the first bend, loads should be raised using vehicle suspension to allow oversail of the safety barrier on the western verge and the fence on the eastern verge. Vegetation should be cleared from both sides of the bend. On the western verge, one set of chevron signs should be removed.
		Through the second bend, loads will oversail both verges. On the western verge, vegetation should be cleared. On the eastern verge vegetation should be trimmed.
		Swept path assessment SK19 is included in Appendix B.



POI	Key Constraint	Details
27	A701 Smid Hope Burn	Loads will continue north on the A701 through a right bend.
		Loads will oversail both verges through the bend.
		On the eastern verge, trees and vegetation should be cleared.
		On the western verge, one bollard and a section of safety barrier will be oversailed. Vegetation should be cleared.
		Swept path drawing SK22 is included in Appendix B.
28	A701 Smid Hope Burn	Loads will continue north on the A701 through a left bend.
		Loads will oversail both verges through the bend. On the right- hand verge, trees should be trimmed. On both verges, vegetation should be trimmed.
		Swept path drawing SK23 is included in Appendix B.
20	AZ01 Fingland	Loads will continue north on the A701 through a right bend
29		Loads will oversail both verges through the bend. On the western verge, one tree should be removed. On the eastern verge two bollards should be removed. Vegetation should be trimmed.
		The vertical profile of the road at this location is pronounced and should be reviewed during the test run stage to ascertain if tar wedges will be required to prevent grounding.
		Swept path drawing SK24 is included in Appendix B.
30	A701 East of Risingclaw Heights	Loads will continue north at the junction.
	The second second	Vehicles will oversail the western verge on approach to the junction where vegetation should be trimmed.
		A series of bollards on the eastern verge of the junction should be removed. Vegetation should be cleared.
		Upon leaving the junction, a series of bollards in the western verge should be removed.
		Swept path drawing SK25 is included in Appendix B.
31	A701 West of Menzion Bend	Loads will cross the bridge heading east on the A701.
		The OS mapping does not show the road edge. An indicative road edge has been provided for illustration only and mitigation should be confirmed on a topographical base survey or during the test run.
		Loads will oversail the southern verge of the A701 through this section. The bridge parapet will be oversailed and one utility pole should be removed.
		Swept path drawing SK26 is included in Appendix B.

POI	Key Constraint	Details
32	Proposed Access Junction	Loads will turn left at the proposed site access junction.
		The junction should be upgraded to meet turbine manufacturer and road authority standards. Detailed design is required. Third party land is required.
		Tree canopy trimming and relocating telephone lines in advance of the junction will be required

3.4 Swept Path Assessment Results and Summary

The detailed swept path drawings for the locations assessed are provided in Appendix B and illustrate tracking undertaken for the worst-case loads.

The colours illustrated on the swept paths are:

- Grey / Black OS / Topographical Base Mapping;
- Green Vehicle body outline (body swept path);
- Red Tracked pathway of the wheels (wheel swept path); and
- Purple The over-sail tracked path of the load where it encroaches out with the trailer (load swept path).

Where mitigation works are required, the extents of over-run and over-sail areas are illustrated on the swept path drawings.

Please note that where assessments have been undertaken using Ordnance Survey (OS) base mapping, there can be errors in the data source. The available OS mapping does not identify the road edge through some sections. An indicative road edge has been provided for illustration only and should be confirmed through a test run or the completion of a topographical survey at these locations.

Where provided by the client, topographical data has been utilised. Please note that PF cannot accept liability for errors on the data source, be that OS base mapping or client supplied data.

3.5 Land Ownership

The limits of road adoption can vary depending upon the location of the site and the history of the roads agency. The adopted area is generally defined as land contained within a defined boundary where the road agency holds the maintenance rights for the land from the original landowner. In urban areas, this is usually defined as the area from the edge of the footway across the road to the opposing footway back edge.

In rural areas the area of adoption can be open to greater interpretation as defined boundaries may not be readily visible. In these locations, the general rule is that the area of adoption is between established fence / hedge lines or a maximum 2m from the road edge. This can vary between areas and location.

3.6 Summary Issues

It is strongly suggested that following a review of the RSR, the Applicant should undertake the following prior to the delivery of the first abnormal loads, to ensure load and road user safety:

- That any necessary topographical surveys are undertaken, and the swept path results repeated to assist in land negotiations, detailed design works, and permitting;
- A revised review of axle loading on structures along the entire access route with the various road agencies is undertaken immediately prior to the loads being transported in case of last-minute changes to structures;
- A review of clear heights with utility providers and the transport agencies along the route to ensure that there is sufficient space to allow for loads plus sufficient flashover protection (to electrical installations);
- That any verge vegetation and tree canopies which may foul loads is trimmed prior to loads moving;
- That a review of potential roadworks and or closures is undertaken once the delivery schedule is established in draft form;
- That a test run is completed to confirm the route and review any vertical clearance issues; and
- That a condition survey is undertaken to ascertain the extents of road defects prior to loads commencing to protect from spurious damage claims.

4 Summary

4.1 Summary of Access Review

PF has been commissioned by Oliver Forest Wind Farm Limited (the Applicant) to prepare a Route Survey Report to examine the issues associated with the transport of AIL turbine components associated with the construction and development of Oliver Forest Wind Farm, located in the Tweed Valley in the Southern Uplands of Scotland.

This report identifies the key points and issues associated with the proposed route and outlines the issues that will need to be considered for successful delivery of components.

The report is presented to the Applicant for consideration. Various road modifications and interventions are required to successfully access the site. If these are undertaken, access to the consented wind farm site is considered feasible.

4.2 Further Actions

The following actions are recommended to pursue the transport and access issues further:

- Undertake detailed design for the proposed new track at the access junction;
- Obtain landowner agreements where third party land is required;
- Undertake the required topographical surveys and repeat the swept path assessments where required;
- Prepare detailed mitigation design proposals to help inform the land option / consultee discussions;
- Obtain the necessary land options (if required);
- Undertake discussion with the affected utility providers and roads agencies;
- Obtain the necessary statutory licences to enable the mitigation measures; and
- Develop a detailed operational Transport Management plan to assist in transporting the proposed loads.

Appendix A Points of Interest Locations

An electronic version of the POI plan can be found here:

https://www.google.com/maps/d/edit?mid=1QyWNQMLpMS5_7BD5z6zUHWDvRygh4yI&usp=sharing













Appendix B Swept Path Assessments

Blade		Tower	
Pel	I Frischmann 93 GEORGE STREET. EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270	Project Oliver Forest Wind Farm	Drawn
Client	Email: pfedinburgh@pellfischmann.com www.pellfischmann.com	Drawing Title	Checked
	Statkraft UK Limited	Nordex N163 Blade and Tower	Drawing No.
Wheel SPA Body SF	A Load SPA Indicative Over-run Over-sail	SPA Location Kings Inch Drive Roundabout 1	SK01

								/		
	Λ									
	ł									
	\	$\langle \rangle$								
	/ /									
_ /	΄ / Γ		~							
	/ /									
	/									
	/									
									\searrow	
	/									
/	/									
							©	Pell	Frisch	imann
Name	Date	Scale			1:500	@ A.	3			
GB	26/09/2023	File No.	912 Oliv	ver	Forest	SPA	N10	63 F	FINAL.d	wa
GB	26/09/2023	Drawing	Statue							
est	1		510105		Dro	oft				
Notes: 1. All mitig	ation is subject to c	onfirmation th	rough a te	st rue	 1.			F	Revision)
2. This is n 3. Indicative confirmed	ot a construction dr road edges have b d through a test run	awing and is een provided f or topograph	intended fo or illustrati ical survey	or illu ion or	 stration pu nly and sh	rposes ould be	only.		XXX	<

	Two road signs to be removed.	
	Existing load bearing surface to be utilised.	
	Project	
Pell Frischmann 93 GEORGE STREET, EDINBURGH, EH2 3ES Tei: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com	Oliver Forest Wind Farm	Drawn Designed Checked
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Intere
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location Kings Inch Drive Roundabout 1	SK01A

		C	Pell Frischmann
Name	Date	Scale 1:500 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	1	Draft	
Notes:			Revision
 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 			XXX

Blade	Tower	
		/
		/
)
Pell Frischmann	Project	
93 GEORGE STREET, EDINBURGH, EH2 3ES	Oliver Forest Wind Farm	Drawn
Email: pfedinburgh@pellfrischmann.com		Checked
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Intere
Key /////	SPA Location	SK02

/				
$\langle \langle \rangle$				
	L			
$\langle \langle \rangle \rangle$				
$\langle \langle \rangle$				
$\langle \langle \rangle$				
\rightarrow				
	$\langle \langle \rangle \rangle$			
6		$\langle \rangle$		
		/ / /		
	2	$\langle \langle \rangle \rangle$		
			$\langle \langle \rangle \rangle$	
		 		
			$\langle \rangle / \rangle$	
			$\langle \rangle \langle \rangle$	
				$\langle \rangle \langle \rangle$
			©	Pell Frischmann
Name	Date	Scale	1:500 @ 43	
AD	26/09/2023	File Ne		
GB	26/09/2023	230912 Oliver	Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	D	
st	2	-	Draft	
Notes:	ation is subject to	onfirmation through a test		Revision
2. This is r	not a construction dr	awing and is intended for illustration	nstration purposes only.	XXX
confirme	d through a test run	or topographical survey.	nıy unu snoula bê	

	$\langle \rangle$	C	Pell Frischmann	
Name	Date	Scale 1:500 @ A3		
AD	26/09/2023			
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg	
GB	26/09/2023	Drawina Status		
est	2	Draft		
Notes:			Revision	
1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.				

Blade		Tower	
	Project		© Pell Frischmann
Pell Frischmann 93 GEORGE STREET, EDINBURGH, EH2 3ES Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com	Oliver Forest Wind Farm	Drawn Designed	AD 26/09/2023 1:500 @ A3 GB 26/09/2023 File 230912 Oliver Forest SPA N163 FINAL.dwg
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Checked Point of Intere Drawing No.	GB 26/09/2023 Drawing Status st 3 Draft Notes: Revision
Key	SPA Location Kings Inch Drive Roundabout 3	SK03	1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only. 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.

		C	Pell Frischmann
Name	Date	Scale 1:500 @ A3	
AD	26/09/2023		
GB	26/09/2023	230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
st	3	Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	ition is subject to c ot a construction dr road edges have b d through a test run	onfirmation through a test run. awing and is intended for illustration purposes only. een provided for illustration only and should be or topographical survey.	XXX

Blade	Tower	
Pell Frischmann	Project	© Pell Frischmann Name Date Scale 1:500 @ A3
93 GEORGE STREET, EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270 Ernail: pfedinburgh@pellfrischmann.com	Oliver Forest Wind Farm	DrawnAD26/09/2023DesignedGB26/09/2023FileNo230912Oliver Forest SPA N163
Client Statkraft IIK Limitad	Drawing Title	Checked GB 26/09/2023 Drawing Status Draft Point of Interest 4 Draft Draft Draft
Statkian OK Linnited Key ////////////////////////////////////	Nordex N163 Blade and Tower	Drawing No. Notes: SKO4 1. All mitigation is subject to confirmation through a test run.
Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location Kings Inch Drive / Mayo Avenue Junction	JNU+ [2. Ihis is not a construction drawing and is intended for illustration purposes only. J. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.

		©	Pell Frischmann				
Name	Date	Scale 1:500 @ A3					
AD	26/09/2023	1.000 0 7.0					
GB	26/09/2023	⁻ ile No 230912 Oliver Forest SPA N163 FINAL.dwg					
GB	26/09/2023	Drawing Status					
est	4	Draft					
Notes:	ntion is subject to c	onfirmation through a test run	Revision				
2. This is r 3. Indicative confirme	not a construction dr road edges have b d through a test rur	wing and is intended for illustration purposes only. sen provided for illustration only and should be or topographical survey.	XXX				



		/ /				
		/ /				
		\sim				
TTTT						
<i>41////</i>	TT					
~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~	11mm					1
		IIm				
						I
					C	DPell Frischmann
Name	Date	Scale		1:500	© A3	DPell Frischmann
Name	Date 26/09/2023	Scale		1:500	© A3	Pell Frischmann
Name AD GB	Date 26/09/2023 26/09/2023	Scale File No.0912	Oliver	1:500 Forest	@ A3 SPA N1	D Pell Frischmann 163 FINAL.dwg
Name AD GB GB	Date 26/09/2023 26/09/2023 26/09/2023	Scale File Maga12 Drawing Stat	Oliver	1:500 Forest	© A3 SPA N1	Pell Frischmann 163 FINAL.dwg
Name AD GB GB est	Date 26/09/2023 26/09/2023 26/09/2023 4	Scale File No0912 Drawing Stat	Oliver	1:500 Forest	© A3 SPA N1	Pell Frischmann 163 FINAL.dwg
Name AD GB GB est Notes:	Date 26/09/2023 26/09/2023 26/09/2023 4	Scale File Monopological Drawing Stat	Oliver	1:500 Forest Dro	© A3 SPA N1 oft	Pell Frischmann 163 FINAL.dwg
Name AD GB GB est Notes: 1. All mitig	Date 26/09/2023 26/09/2023 26/09/2023 4	Scale File No. Drawing Stat	Oliver US	1:500 Forest Dro	© A3 SPA N1 oft	D Pell Frischmann 163 FINAL.dwg Revision
Name AD GB GB est Notes: 1. All mitiga 2. This is n 3. Indicative	Date 26/09/2023 26/09/2023 26/09/2023 4 tion is subject to c of a construction of road edges have b	Scale File No 230912 Drawing Stat	Oliver US a test run et for illu stration ou	1:500 Forest Dro	© A3 SPA N1 oft	Pell Frischmann 163 FINAL.dwg Revision XXX



								Nordex N163 Blade and lower	Drawin
Kev					77777				
,							SPA Location		SK SK
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail		M8 Slip Road	



Plada	Tower	
Blade	Tower	
Pell Frischmann 93 George Street, Edinburgh, EH2 3ES Tei: +44 (0)131 240 1270	Project Oliver Forest Wind Farm	Drawn Designed
Email: ptedinburgn@pellfrischmann.com www.pellfrischmann.com	Drawing Title	Checked
Statkraft UK Limited	Nordex N163 Blade and Tower	Point of Intere Drawing No.
Key Wheel SPA Body SPA Load SPA Indicative Over-run O	ver-sail A74(M) Junction 15 / A701 Roundabout	SK06

			©	Pell Frischmann				
Name		Date	Scale 1:1000 @ 43					
,	AD	26/09/2023						
GB		26/09/2023	File No. 230912 Oliver Forest SPA N1	63 FINAL.dwg				
GB		26/09/2023	Drawing Status					
est		6	- Draft					
No 1.	otes: All mitigo	ntion is subject to c	onfirmation through a test run.	Revision				
2. 3.	This is n Indicative confirmed	ot a construction dr road edges have b d through a test run	awing and is intended for illustration purposes only. een provided for illustration only and should be or topographical survey.	XXX				

Load bearing surface to be laid. One road sign, one lighting column and fence to be removed. Trees and vegetation to be cleared. Land to be reprofiled to allow oversail.

One lighting column, one road sign and fence to be removed. Safety barrier and junction box to be oversailed. Trees and vegetation to be cleared. Land to be reprofiled to allow blade tip oversail. Topographical survey required to confirm mitigation measures.

- Two bollards to be removed.

 Load bearing surface to be laid. One set of chevron signs to be removed.

											<u> </u>		C	Pell Frischmann
		Pell	Frisch	man	n		Project				Name	Date	Scale 1:500 @ A3	
		1 011	93 GEORGE STREET	, EDINBURGH. EH2 3I	ES			Oliver Forest Wind Forms		Drawn	AD	26/09/2023		
			Te Email: ofedinburah	l: +44 (0)131 240 127 @pellfrischmann.co	70 2m			Unver Forest wind Farm		Designed	GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
			~~~~	w.pellfrischmann.co	m					Checked	GB	26/09/2023	Drawing Status	
Client Statkraft LIK Limited				Drawing Title	awing Title		Point of Intere	est	6	Draft				
							-	Nordex N163 Blade and lower		Drawing No.	Notes:		-	Revision
Key							SPA Location			SK06A	1. All mitig 2. This is r	ation is subject to not a construction o	confirmation through a test run. Irawing and is intended for illustration purposes only.	
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail		A74(M) Junction 15 / A701 Roundabout			3. Indicative confirme	e road edges have d through a test ru	been provided for illustration only and should be in or topographical survey.	XXX

-Load bearing surface to be laid.

Blade		Tower	
Pell	Frischmann 93 GEORGE STREET, EDINBURGH, EH2 3ES Tel: +44 (0)131 240 1270	Project Oliver Forest Wind Farm	Drawn
	Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com		
Client	Statkraft UK Limited	Drawing Title	Checked Point of Inter

3						
1						
$ \land $						
.\						/
$\sum (\langle V \rangle \langle V \rangle \rangle$						
					©	Pell Frischmann
Name	Date	Scale		1:1000	@ A3	
AD	26/09/2023	File No.				
GB	26/09/2023	230912	Oliver	Forest	SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Sta	tus	Dre	nft	
st	7					Deut-1
Notes: 1. All mitigo	ntion is subject to c	onfirmation through	a test ru	n.		Revision
2. This is n 3. Indicative	ot a construction dr road edges have b	awing and is intend een provided for illu	ed for illu Istration o	stration pu nly and sh	rposes only. ould be	XXX
confirmed	α through a test run	or topographical s	urvey.			


Name	Date	Scale 1:1000 @ 43	
AD	26/09/2023	1.1000 9 //0	
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	7	S Draft	
Notes:			Revision
 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 			XXX



nan /	/Bridg)e///					
	/						
ζ	4						
~/							
	77						
						© Pe	ell Frischmann
Name	Date	Scale		1.1000	_	.7	
AD	26/09/2023	F 1 1:		1:1000	4 yu	.	
GB	26/09/2023	^{File} 2309	12 Oliver	Forest	SPA	N163	FINAL.dwg
GB	26/09/2023	Drawing	Status	Dro	ıft		
est Notes:	8					-	Revision
1. All mitigo 2. This is n	ition is subject to co ot a construction dr	onfirmation thro awina and is in	ough a test ru ntended for illu	n. stration num	poses	only.	
3. Indicative confirmed	road edges have be through a test run	en provided fo or topographi	r illustration o cal survey.	nly and sha	ould be		XXX
_!						1	



Name	Date	Scale 1.1000 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
st	8	S Draft	
Notes:			Revision
 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 			XXX

Blade	Tower	Statue
Pell Frischmann 93 GEORGE STREET, EDINBURGH, EH2 3ES Tel: +44 (0)131 240 1270	Project Oliver Forest Wind Farm	Drawn Designed
Client Statkraft UK Limited	Drawing Title	Checked Point of Intere
Key	SPA Location A701 Moffat	Drawing No. SK09

~		
E		
1/ F		
	2	
	PL 1	
\E\	3//	
12	X E /	
1	N 81+	
1	The la	
		ł
		A
	ss F	
	1.	\mathcal{H}
$\langle \rangle$	NUN	
	12/1	
	EUN	
	18-	
	No Z	
		5 1 1
		3
		3
	SAT	
		R R
		Hove
		© Pell Frischmann
Name	Date	Scale 1:1000 @ A3
AD	26/09/2023	File No
GB	26/09/2023	^{rile} 230912 Oliver Forest SPA N163 FINAL.dwg
GB	26/09/2023	Drawing Status
est	9	
Notes: 1. All mitia	ation is subject to c	onfirmation through a test run.
2. This is r 3. Indicative	not a construction dr road edges have be	awing and is intended for illustration purposes only.
confirme	d through a test run	or topographical survey.

Name	Date	Scale 1:1000 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
rest	9	Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 		

Blade	Tower Tower St M But Chur			
Pell Frischmann 93 GEORGE STREET, EDINBURGH, EH2 3ES Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com	Project Oliver Forest Wind Farm	Drawn Designed Checked	NameAD26GB26GB26	Date 3/09/2023 3/09/2023 6/09/2023
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Intere	st Notes: 1. All mitigation	10 is subject to co
Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location A701 High Street and Academy Road Junction	SK10	 This is not a Indicative roa confirmed th 	construction dra d edges have been rough a test run

Name Date Scale 1:500 @ A3 AD 26/09/2023 File No GB 26/09/2023 File No GB 26/09/2023 Drawing Status Draft erest 10 Draft Revision Notes: 1. All mitigation is subject to confirmation through a test run. Revision 1. All mitigation is subject to confirmation through a test run. XXX 2. This is not a construction drawing and is intended for illustration purposes only. XXX				
Name Date Scale AD 26/09/2023 1:500 @ A3 GB 26/09/2023 File No GB 26/09/2023 Drawing Status Draft erest 10 Draft Notes: 1. All mitigation is subject to confirmation through a test run. Revision 1. All mitigation is not a construction drawing and is intended for illustration purposes only. XXX 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. XXX	N		Scala	Pell Frischmann
KD 20/03/2023 File No GB 26/09/2023 File 230912 Oliver Forest SPA N163 FINAL.dwg GB 26/09/2023 Drawing Status Draft erest 10 Draft Notes: 1. All mitigation is subject to confirmation through a test run. Revision 2. This is not a construction drawing and is intended for illustration only and should be confirmed through a test run or topographical survey. XXX	Name	Date	1:500 @ A3	
CB 26/09/2023 Drawing Status erest 10 Draft Notes: 1. All mitigation is subject to confirmation through a test run. Revision 2. This is not a construction drawing and is intended for illustration purposes only. XXX 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. XXX		20/03/2023	File No. 230912 Oliver Forest SPA N1	63 FINAL dwg
GB 20/09/2023 Drawing Status erest 10 Draft Notes: I. All mitigation is subject to confirmation through a test run. Revision 1. All mitigation is subject to confirmation through a test run. XXX 2. This is not a construction drawing and is intended for illustration purposes only. XXX 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. XXX	GB	20/09/2023	ZOUSTZ UNVER FOREST SPA NT	os rinal.awg
Prest 10 Druit Notes: I. All mitigation is subject to confirmation through a test run. Revision 2. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. XXX	GB	26/09/2023	Drawing Status	
Notes: Revision 1. All mitigation is subject to confirmation through a test run. Revision 2. This is not a construction drawing and is intended for illustration purposes only. XXX 3. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. XXX	erest	10	Draft	
An integration is subject to commination through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.	Notes:	tion in orthight to	onfirmation through a test are	Revision
	2. This is n 3. Indicative confirmed	toon is subject to co ot a construction dr road edges have be d through a test run	arming and is intended for illustration purposes only. een provided for illustration only and should be a or topographical survey.	XXX



		U	Pell Frischmann
Name	Date	Scale 1:500 @ A3	
AD	26/09/2023	1.000 0 7.0	
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	10	5 Draft	
Notes:			Revision
 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 			XXX



		©	Pell Frischmann
Name	Date	Scale 1:1000 @ A3	
AD	26/09/2023	File No. 12 Oliver Forest SPA NI	63 FINAL dwg
GR CR	20/09/2023	200912 UNVER FOREST SPA NI	00 FINAL.OWG
est	11	Drawing Status Draft	
Notes:			Revision
 All mitiga This is n Indicative confirmed 	tion is subject to c ot a construction dr road edges have b I through a test run	onfirmation through a test run. awing and is intended for illustration purposes only. sen provided for illustration only and should be or topographical survey.	XXX

	Pell Frischmann 93 GEORGE STREET, EDINBURGH. EH2 3ES	Project Oliver Forest Wind Farm	Drawn
	Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com		Designed Checked
Client	Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Inter
Key	— — <i>— //////</i>	SPA Location	Urawing No. SK11A

		1				
					C	Pell Frischmann
Name	Date	Scale	1:5	00	@ A3	
AD GB	26/09/2023 26/09/2023	File No 230912	Oliver For	est	SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Sta	tus	Dre	nft	-
Notes:	11				uit	Revision
1. All mitig 2. This is i	ation is subject to co not a construction dr	onfirmation through awing and is intend	a test run. ed for illustratio	on pu	rposes only.	XXX
confirme	d through a test run	or topographical s	urvey.	u SN	ouid De	



Name Date AD 26/09/2023	Scale	1:1250	@ A3	© Pell Fri	schmann
AD 20/09/2023 GB 26/09/2023	File No 230912	Oliver Forest	SPA N	163 FINA	L.dwa
GB 26/09/2023	200812	Silver FULESL	JPA N	I UU PINA	L.uwy
120/09/2023	Drawing Stat	us Dro	ıft		
Notes: 1. All mitigation is subject to cr 2. This is not a construction dr 3. Indicative road edges have be confirmed through a test run	onfirmation through awing and is intende een provided for illus or topographical su	a test run. sd for illustration pu stration only and shu urvey.	rposes on build be	Revis	ion XX

Series of bollards to be oversailed.
 Trees and vegetation to be cleared.
 Third party land required.

Series of bollards to be oversailed. Fence and – gate to be removed. Trees and vegetation to be cleared. **Third party land** required.

> Load bearing surface to be laid. One utility pole to be removed. One bollard and fence to be oversailed. Third party land required.

Trees and vegetation to be removed.

												U U	Pell Frischmann
		Pell	Frisch	nmanı	n		Project			Name	Date	Scale 1:1500 @ A3	
		1.011	93 GEORGE STREE	ET, EDINBURGH. EH2 3	E ● BES			Oliver French Wind French	Drawn	AD	26/09/2023		
			T Email: pfedinburg	el: +44 (0)131 240 127 h@pellfrischmann.co	70 om			Uliver Forest Wind Farm	Designed	GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
			· •••	vw.pellfrischmann.co	m				Checked	GB	26/09/2023	Drawing Status	
Client			Statkraft l	JK Limited			Drawing Title		Point of Inte	erest	12	Draft	
							-	Nordex N163 Blade and lower	Drawing No.	Notes:			Revision
Key							SPA Location		SK12A	1. All mit 2. This is	igation is subject to c not a construction dr	onfirmation through a test run. awing and is intended for illustration purposes only.	XXX
	Wheel SPA	Body SPA	Load SPA	Indicative	Over-run	Over-sail		A/U1 Mottat Cemetery		confirm	ve road eages have b ned through a test rur	een provided for illustration only and should be a or topographical survey.	/////



		C	Pell Frischmann
Name	Date	Scale 1:500 @ A3	
AD	26/09/2023		
GB 26/09/2023		File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
st	13	5 Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	ation is subject to c lot a construction dr road edges have b d through a test rur	onfirmation through a test run. awing and is intended for illustration purposes only. een provided for illustration only and should be or topographical survey.	XXX



		Ô	Pell Frischmann
Name	Date	Scale 1:1000 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	13	Draft	
Notes:			Revision
 All mitigo This is n Indicative confirmed 	XXX		

Blade		159.4m			Tower	159.4m			
								¢) Pell Frischmann
	Pell Frischmo	I N N H. EH2 3ES	Project			Drawn	Name Date AD 26/09/2023	Scale 1:1000 @ A3	
	Tel: +44 (0)13 Email: pfedinburgh@pellfrisch www.pellfrisch	1 240 1270 nann.com nann.com		Olíver Forest Wind Farm		Designed	GB 26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
Client	Statkraft UK Limi	ted	Drawing Title			Checked Point of Intere	GB 26/09/2023 est 14	Drawing Status Draft	
			_	Nordex N163 Blade and Tower		Drawing No.	Notes: 1. All mitigation is subject to a	confirmation through a test run.	Revision
Wheel SPA	Body SPA Load SPA Indica	tive Over-run Over-sail	SPA Location	A701 Bends South of Adamsholm		5K14	 This is not a construction d Indicative road edges have t confirmed through a test ru 	trawing and is intended for illustration purposes only, been provided for illustration only and should be in or topographical survey.	XXX



Name Date		Scale 1:1000 @ A3		
AD	26/09/2023			
GB 26/09/2023		File No 230912 Oliver Forest SPA N1	63 FINAL.dwg	
GB 26/09/2023 est 14		Drawing Status Draft		
1. All mitigo 2. This is n 3. Indicative confirmed	XXX			



	Зm			
			©	Pell Frischmann
Name Date	Scale	1:1000	@ A3	
AD 26/09/2023	File Nonto o	Nivor Forst		
GB 26/09/2023	230912 0	niver Forest	SPA NIC	55 FINAL.OWG
GB 20/09/2023	Drawing Statu	s Dro	ıft	
Notes:				Revision
 All mitigation is subject to cr This is not a construction dr Indicative road edges have be confirmed through a test run 	onfirmation through a awing and is intended sen provided for illustr or topographical surv	test run. for illustration pur ration only and sho rey.	rposes only. build be	XXX



Name	Date	Scale 1:1000 @ A3			
AD	26/09/2023				
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg		
GB 26/09/2023 est 15		Drawing Status			
		S Draft			
Notes:	ation in subject to a	antimation through a test run	Revision		
 This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 			XXX		

Pell Frischmann Project Drawn Drawn Designed Drawn End: pfedinburgh@pellischmann.com Drawing Title Client Statkraft UK Limited Key Frage Key SPA Location	Blade		35m	Tower	
Key Mordex Nordex N163 Blade and Tower Key Image: Station Image: Station SK16	Pell Fri 93 GE Email: Client Ctat	ischmann Eorge Street, EDINBURGH, EH2 3ES Tel: +44 (0)131 240 1270 : pfedinburgh@pellfrischmann.com www.pellfrischmann.com	Project Oliver Forest Wind Farm Drawing Title	<u> </u>	Drawn Designed Checked Point of Inter
	Key		SPA Location		Drawing No. SK16

\		``			
\sim	Dall		inak	ma	nn
V Y		\\\	1201	nnu	
```		· · · ·			

Name Date AD 26/09/2023 GB 26/09/2023 GB 26/09/2023 est 16		Date	Scale 1:500 @ A3		
		26/09/2023			
		26/09/2023	File No. 230912 Oliver Forest SPA N163 FINAL.dwg		
		26/09/2023	Drawing Status		
		16	Draft		
	Notes:			Revision	
	 All mitige This is r Indicative confirme 	XXX			

	One road sign to be removed. Vegetation to be trimmed.	
Pell Frischmann 93 GEORGE STREET. EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfischmann.com	Project Oliver Forest Wind Farm	Drawn Designed
Client Statkraft UK Limited	Drawing Title	Checked Point of Inter
Key /////	Nordex NIDS Blade and lower	Drawing No.
Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	A701/B719 Junction	

	Ŭ	
Date	Scale 1:500 @ A3	
26/09/2023		
26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
26/09/2023	Drawing Status	
16	Draft	
		Revision
ation is subject to c not a construction dr e road edges have b ed through a test rur	onfirmation through a test run. awing and is intended for illustration purposes only. een provided for illustration only and should be a or topographical survey.	XXX
	Date 26/09/2023 26/09/2023 26/09/2023 16 ation is subject to c not a construction dr e road edges have b d through a test rur	Date Scale 1:500 @ A3 26/09/2023 File No 26/09/2023 File 230912 Oliver Forest SPA N1 26/09/2023 Drawing Status Draft 16 Draft ation is subject to confirmation through a test run. not a construction drawing and is intended for illustration only and should be d through a test run or topographical survey.

Blade 24	5 m	Tower	45m		
					Pell Frischmann
Pell Frischmann 93 George Street, edinburgh, eH2 3es	Project Oliver Forest Wind Form		Name Drawn AD	Date Scale 1:1000 @ A3 26/09/2023 File Ne	
Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com			Designed GB Checked GB	26/09/2023 ^{rile} 230912 Oliver Forest SPA N10 26/09/2023 Drawing Status	63 FINAL.dwg
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower		Point of Interest Drawing No. Notes:	17 Draft	Revision
Key	SPA Location A701 Holehouse Bridge and Bend	İs	SK17 1. All mitig 2. This is 3. Indicativ confirme	gation is subject to confirmation through a test run. not a construction drawing and is intended for illustration purposes only. re road edges have been provided for illustration only and should be ed through a test run or topographical survey.	XXX

	Series of bollards to be oversailed. Vegetation to be trimmed.	+		
Bridge parapet and safety barrier to be oversailed. Two sets of chevron signs to be removed. Trees and vegetation to be cleared. Third party land required.	Bridge parapet and safety barrier to be oversailed. Trees and vegetation to be cleared. Third party land required.	One road safety bar	sign to be remove rrier to be oversail	ed.
Pell Frischmann 93 george street, edinburgh, eH2 3es	Project Oliver Forest Wind Farm	Drawn	Name Date AD 26/09/2023	© Pell Frischmann Scale 1:1000 @ A3
Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Designed Checked Point of Interes	GB 26/09/2023 GB 26/09/2023 st 17	Drawing Status Draft
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location A701 Holehouse Bridge and Bends	SK17A	 All mitigation is subject to c This is not a construction dr Indicative road edges have b confirmed through a test rur 	onfirmation through a test run. awing and is intended for illustration purposes only. een provided for illustration only and should be or topographical survey.

Blade	Tower
Pell Frischmann Project Diver Forest Wind Farm	Image: Solution of the second sec
Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfischmann.com Oliver Forest Wind Farm Client Statkraft UK Limited Key Orawing Title SPA Location AZO1 Weath of Organization	Designed GB 26/09/2023 File No Checked GB 26/09/2023 Drawing Status Draft Point of Interest 18 Draft Revision Drawing No. Notes: . . Revision SK18 1. All mitigation is subject to confirmation through a test run. . . XXX

Vegetation to be trimme	ed.	
Pell Frischmann 93 GEORGE STREET, EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com	Project Oliver Forest Wind Farm	Drawn Designed
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Checked Point of Inter Drawing No.
Key	SPA Location A701 West of Granton	SK18A

Name	Date	Scale 1:500 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	18	S Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	ation is subject to c ot a construction dr road edges have b d through a test rur	onfirmation through a test run. awing and is intended for illustration purposes only. sen provided for illustration only and should be or topographical survey.	XXX

Blade		Tower	
	Deniant		© Pell Frischmann
Pell Frischmann	Project		Name Date Scale Custom @ A3
93 GEORGE STREET, EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270		Oliver Forest Wind Farm	Designed GB 26/09/2023 File No 230912 Oliver Forest SPA N163 FINAL dwg
Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com			Checked GB 26/09/2023 Drawing Status
Client Statkraft LIK Limited	Drawing Title		Point of Interest 20 & 21 Drawing Status Draft
		Nordex N163 Blade and Tower	Drawing No. Notes:
Key	SPA Location		SK19 1. All mitigation is subject to confirmation through a test run. 2. This is not a construction drawing and is intended for illustration purposes only.
Wheel SPA Body SPA Load SPA Indicative	Over-run Over-sail	A701 Auldhousehill Bridge	 Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey.
		~	



Name	Date	Scale 1.1500 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	20 & 21	Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	ation is subject to c ot a construction dr road edges have b d through a test rur	onfirmation through a test run. awing and is intended for illustration purposes only. sen provided for illustration only and should be or topographical survey.	XXX



© Pell	Frischmann
--------	------------

Name	Date	Scale 1:2000 @ 43	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	22–25	S Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	ation is subject to c lot a construction dr road edges have b d through a test rur	onfirmation through a test run. awing and is intended for illustration purposes only. sen provided for illustration only and should be or topographical survey.	XXX



© Pell	Frischmann
--------	------------

Name	Date	Scale 1:2000 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	22–25	S Draft	
Notes:			Revision
 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 		XXX	



One set of chevron signs to be removed. Safety barrier to be oversailed. Vegetation to be cleared. **Third party land** required.

One set of chevron signs to be removed.
 Safety barrier to be oversailed. Vegetation
 to be cleared. Third party land required.

Name	Date	Scale 1:2000 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
st	22–25	S Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	ation is subject to c tot a construction dr road edges have b d through a test rur	onfirmation through a test run. awing and is intended for illustration purposes only. een provided for illustration only and should be a or topographical survey.	XXX

Blode	Tower	12p
Pell Frischmann	Project	
93 GEORGE STREET, EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270	Oliver Forest Wind Farm	Drawn Designed
Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com		Checked
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Inter
Key /////	SPA Location	SK21
Wheel SPA Body SPA Load SPA Indicative Over-run Ov	er-sail A701 Old Road Bends	

Name AD GB GB est Notes: 1. All mitigat	Date 26/09/2023 26/09/2023 26/09/2023 26	Scale File No. Drawing Status	© 1:1000 @ A3 Forest SPA N1 Draft un.	Pell Frischmann 63 FINAL.dwg Revision

Three utility poles to be removed. Vegetat to be clear	ion ed. + 412m	
I I	Oliver Forest Wind Farm	Drawn Designed
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Inter
Key /////	SPA Location	Drawing No.
	A701 Old Road Bends	

Name	Date	Scale 1.1000 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
est	26	S Draft	
Notes:			Revision
 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 		XXX	



		~			
				©	Pell Frischmann
Name	Date	Scale	1:1000	@ A3	
AD	26/09/2023				
GB	26/09/2023	^{rile} 230912 Oliver	Forest	SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawina Status			
est	27		Dro	oft	
Notes:	•				Revision
1. All mitige 2. This is r 3. Indicative confirme	ation is subject to c not a construction dr e road edges have b d through a test rur	onfirmation through a test m awing and is intended for ill een provided for illustration of or topographical survey.	un. ustration pur only and sho	rposes only. build be	XXX

	One bollard and saf oversailed. Vegetation	346m	
	Pell Frischmann 93 George Street, eDINBURGH. EH2 365	Project Oliver Forest Wind Farm	Drawn
	Tel: +44 (0)131 240 1270		Designed
	Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com		Checked
Client	Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com Statkraft UJK Limited	Drawing Title	Checked Point of Interes
Client	Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com	Drawing Title Nordex N163 Blade and Tower	Checked Point of Interes Drawing No.

		\			
		/ / / /			
					_
					/
					/
					/
			/	Ø	Pell Frischmann
Name	Date	Scale	1.1000	© @ 43	^p ell Frischmann
Name AD	Date 26/09/2023	Scale	1:1000	© F @ A3	² ell Frischmann
Name AD GB	Date 26/09/2023 26/09/2023	Scale File No. 230912 C	1:1000 Dliver Forest	© F @ A3 SPA N16	Pell Frischmann 3 FINAL.dwg
Name AD GB GB	Date 26/09/2023 26/09/2023 26/09/2023	Scale File No. 230912 C	1:1000 Niver Forest	© 1 @ A3 SPA N16	² ell Frischmann 3 FINAL.dwg
Name AD GB GB	Date 26/09/2023 26/09/2023 26/09/2023 27	Scale File No. 230912 C Drawing Statu	1:1000 Diver Forest s Dro	© I @ A3 SPA N16 ift	Pell Frischmann 3 FINAL.dwg
Name AD GB GB st	Date 26/09/2023 26/09/2023 26/09/2023 27	Scale File No. 230912 C Drawing Statu	1:1000 Diver Forest s Dro	© F @ A3 SPA N16 ift	Pell Frischmann 3 FINAL.dwg Revision
Name AD GB GB st Notes: 1. All mitiga	Date 26/09/2023 26/09/2023 26/09/2023 27 tion is subject to c	Scale File No. 230912 C Drawing Statu	1:1000 Diver Forest S Drc	© F @ A3 SPA N16 ift	Pell Frischmann 3 FINAL.dwg Revision
Name AD GB GB st Notes: 1. All mitiga 2. This is n 3. Indicative	Date 26/09/2023 26/09/2023 26/09/2023 27 tion is subject to c ot a construction dr road edges have b	Scale File No 230912 C Drawing Statu	1:1000 Diver Forest S Drc test run. for illustration pur ation only and sha	© F @ A3 SPA N16 ift	Pell Frischmann 3 FINAL.dwg Revision XXX

Blade			Tower	
Pall	Frischmann	Project		
	93 GEORGE STREET. EDINBURGH. EH2 36S Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com	Oliver Forest Wind Farm		Drawn Designed Checked
Client	Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower		Point of Intere
Wheel SPA Body SPA	Load SPA Indicative Over-run Over-sail	SPA Location A701 Smid Hope Burn		SK23

		338m
Name	Date	© Pell Frischmann
AD	26/09/2023	1:1000 @ A3
	20/03/2023	File No. 12 Oliver Ferret SDA N167 FINAL due
GB	26/09/2023	ZOUYTZ UNVER FOREST SPA NIBO FINAL.dwg
GB	26/09/2023	Drawing Status
est	28	Draft
Notes: 1. All mitiga 2. This is n 3. Indicative confirmed	ition is subject to c ot a construction dr road edges have b d through a test run	onfirmation through a test run. awing and is intended for illustration purposes only. sen provided for illustration only and should be or topographical survey.

	Vegetation to be trimmed. Trees and vegetation to be trim	med.				
		ŢŢ		1	Conte	© Pell Frischmann
Pell Frischmann 93 george street, edinburgh. eH2 3es	Oliver Forest Wind Farm	Drawn	Name AD	Date 26/09/2023	1:1000 @ A3	
Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com		Designed Checked	GB GB	26/09/2023 26/09/2023	3 230912 Oliver Forest SPA 3 Drawing Status	N163 FINAL.dwg
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Inte	erest Notes:	28	Draft	Revision
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location A701 Smid Hope Burn	SK23A	1. All mitig 2. This is 3. Indicative confirme	gation is subject to not a construction o re road edges have l ed through a test ru	confirmation through a test run. drawing and is intended for illustration purposes o been provided for illustration only and should be un or topographical survey.	nty. XXX
	1	1				_ _

Blade	Tower	
Pell Frischmann	Project	
93 GEORGE STREET, EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270 Email: pfedinburgh@pellfrischmann.com www.pellfrischmann.com	Oliver Forest Wind Farm	Drawn Designed Checked
Client Statkraft UK Limited	Drawing Title Nordex N163 Blade and Tower	Point of Intere Drawina No.
Key Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location A701 Fingland	SK24
	-	

	Name	Date	Scale 1:500 @ A3		
	AD	26/09/2023			
	GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg	
	GB	26/09/2023	Drawing Status		
est 29		29	Draft		
	Notes:		Revision		
	 All mitigo This is n Indicative confirmed 	XXX			



Name	Date	Scale 1:1000 @ A3		
AD	26/09/2023			
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg	
GB	26/09/2023	Drawing Status		
est	29	S Draft		
Notes:			Revision	
 All mitiga This is n Indicative confirmed 	XXX			
Blode	Tower 30			
--	--	-----------------		
Pell Frischmann	Project	Drawn		
93 GEORGE STREET. EDINBURGH. EH2 3ES Tel: +44 (0)131 240 1270	Oliver Forest Wind Farm	Designed		
Email: ptedinburgh@pellfrischmann.com www.pellfrischmann.com		Checked		
Client Statkraft UK Limited	Drawing Title	Point of Intere		
		Drawing No.		
Wheel SPA Body SPA Load SPA Indicative Over-run Over-sail	SPA Location A701 East of Risingclaw Heights	SK25		

Scale Name Date 1:1000 @ A3 26/09/2023 26/09/2023 File No 230912 Oliver Forest SPA N163 FINAL.dwg AD GB 26/09/2023 Drawing Status GB Draft 30 est Notes: Revision All mitigation is subject to confirmation through a test run.
 This is not a construction drawing and is intended for illustration purposes only.
 Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. XXX

© Pell Frischmann



© Pell Frischmann

Name	Date	Scale 1.1000 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No. 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
st	30	S Draft	
Notes:			Revision
1. All mitigo 2. This is n 3. Indicative confirmed	ation is subject to c lot a construction dr road edges have b d through a test rur	onfirmation through a test run. awing and is intended for illustration purposes only. sen provided for illustration only and should be or topographical survey.	XXX

Blade +	2 m	Tower	
		Project	
Pell Fri	S C h m a n n PRGE STREET, EDINBURGH, EH2 3ES	Oliver Forest Wind Farm	nwc
Email: p	Tel: +44 (0)131 240 1270 ofedinburgh@pellfrischmann.com www.pellfrischmann.com	Des	signed ecked
Client Stat	kraft UK Limited	Drawing Title Poi	int of Inte
Kau	F777777 F777777	Nordex N163 Blade and lower	wing No.
Wheel SPA Body SPA Load	SPA Indicative Over-run Over-sail	SPA Location A701 West of Menzion Bend	SK26



© Pell Frischmann

Name	Date	Scale 1:750 @ A3	
AD	26/09/2023		
GB	26/09/2023	File No 230912 Oliver Forest SPA N1	63 FINAL.dwg
GB	26/09/2023	Drawing Status	
erest	31	S Draft	
Notes:			Revision
 All mitigation is subject to confirmation through a test run. This is not a construction drawing and is intended for illustration purposes only. Indicative road edges have been provided for illustration only and should be confirmed through a test run or topographical survey. 			XXX

+	
Pell Frischmann Project 93 GEORGE STREET. EDINBURGH. EH2 3ES Drawn AD 26/09/2023	
Tel: +44 (0)131 240 1270 Email: pr6dinbyge@pellfrischmann.com	53 FINAL.dwg
www.pellfischmann.com Checked GB 26/09/2023 Drawing Status	
Client Statkraft UK Limited Nordex N163 Blade and Tower	Revision
Key Image: All mitigation is subject to confirmation through a test run. Wheel SPA Body SPA Load SPA Indicative Over-run Over-run Over-sail SPA Location A701 West of Menzion Bend	XXX